



WISCONSIN'S PREMIER 1/3 MILE CLAY RACE TRACK

460 E. County Rd Y
OSHKOSH, WI 54901



2012

OSHKOSH SPEEDZONE RACEWAY RULE BOOK

Official interpretation of these rules is made by Track Officials and is final. Amendments to these rules may be made at any time at the discretion of the Track Officials. Non-compliance may result in not being able to compete, loss of points, loss of monies, fines, and/or suspensions.

RULES DISCLAIMER

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules. **NO EXPRESS OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM PUBLICATIONS OF OR COMPLIANCE WITH THESE RULES AND/OR REGULATIONS.** They are intended as a guide for conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, officials, or others. The race director shall be empowered to permit minor deviation from any of the specifications herein or impose any further restrictions that in his/her opinion does not alter the minimum acceptable requirements. **NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM SUCH ALTERATION OF SPECIFICATIONS.** Any interpretation or deviation of these rules is left to the discretion of the officials. Their decision is final.

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**IMPACT MOTORSPORTS LLC
D.B.A. OSHKOSH SPEEDZONE RACEWAY
PO BOX 3226
OSHKOSH, WI 54903-3226**

DAY OF EVENT, TRACK NUMBER - 1-920-279-9096 / 920-252-1463
Website - www.oshkoshspeedzone.net
Email – oshkoshspeedzone@yahoo.com

REPRESENTATIVES:

Competition Director –

Modified Rep. – Dennis Baldry – 920-203-0296 (Cell)

Grand National Rep. – Jerry Winkler – 920-203-6455 (Cell)

Street Stock Rep. – Louis Reichenberger – 920-426-0523 (Home)

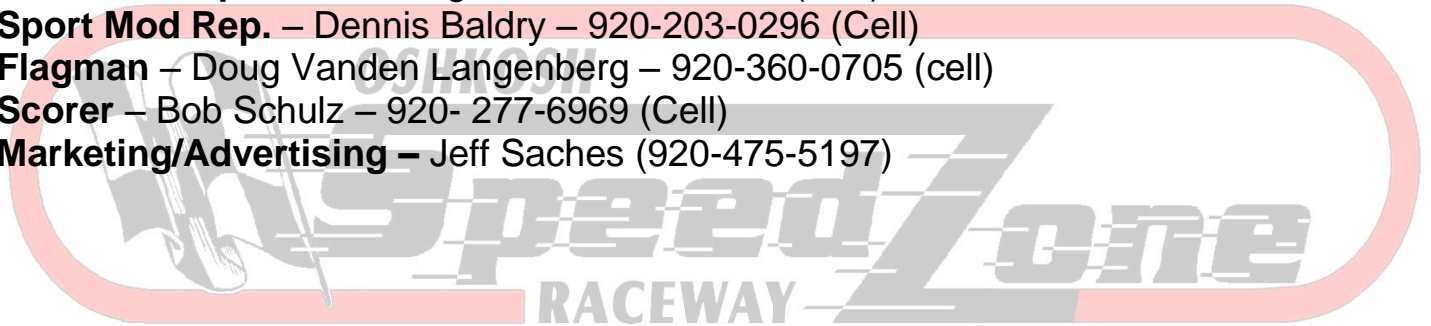
Dirt Devil Rep. – Mike Morgan – 920-427-8373 (Cell)

Sport Mod Rep. – Dennis Baldry – 920-203-0296 (Cell)

Flagman – Doug Vanden Langenberg – 920-360-0705 (cell)

Scorer – Bob Schulz – 920- 277-6969 (Cell)

Marketing/Advertising – Jeff Saches (920-475-5197)



IMPACT MOTORSPORTS LLC – OWNERS

JEFF LEMIESZ – 920-279-9096 (Cell)

LARRY STRATTON – 920-252-1463 (Cell)

REVISIONS/CLARIFICATIONS UNDERLINED

**DIAGRAMS IN THIS BOOKLET ARE FOR REFERENCE ONLY WITH THE
EXCEPTION OF THE MODIFIEDS**

SECTION - 1

2011 OSHKOSH SPEEDZONE RACEWAY OPERATION RULES

1.1 SCHEDULE OF EVENTS

4:00 – Pit gates open.

6:00 – Race car registration deadline for lineups or you will be considered late.

6:00 – Mandatory Drivers Meeting. If not present you will be put in the back of your heat. Roll call may be done.

6:15 – Track Packing if needed – If you do not pack when asked you will start all events in the back row.

6:25 – Opening Ceremonies; National Anthem with parade lap of previous weeks feature winners.

6:30 – Racing starts.

1.2 ANNUAL RELEASE FORM

An annual release form must be filled out prior to admittance to the pit area. Annual release card must be shown prior to admittance to the pit area. If you do not have an annual release form filled out, you will be asked to fill one out and sign the waiver.

1.3 PIT AREA

Anyone under 12 years of age must be accompanied by a parent or guardian to enter the pit area. Anyone under 18 years of age must have an insurance minor release form filled out, signed and on file with Impact Motorsports LLC before admission.

Wrist band must be worn on left wrist at all times. Drivers must purchase and wear a wrist band for each division that they are competing in. If you are caught in pit area without wrist band you will be fined \$50.00 plus suspended from pit area until fine is paid. Racecar driver is the sole spokesman for the car in any and all matters pertaining to an Event, and is responsible for the actions of the pit crew, and owner.

No alcoholic beverages may be opened in the pit area until after the entire program is completed – any violation will result in being ejected from the track, loss of points and/or earnings may also be forfeited.

Proper footwear required in the pit area.

1.3.1 ATV, GOLF CARTS & MOTORCYCLES

ATV's & Golf Carts will be allowed; ATV's and Golf Carts must register with the Speedzone and show proof of insurance. Registration forms will be available online and through Impact Motorsports LLC.

Motorcycles and bicycles are prohibited from the pit area, unless prior arraignments have been made or allowed by touring series.

1.3.2 PIT STALLS

Front row pit stalls can be reserved for \$100 for the season (including specials). Prior rented stalls will be available to previous year's renters until the end of January of the upcoming year then on a first come first paid basis. Any other spot may be reserved (including specials) for \$50. You must supply your own pit stall sign designating your reserved spot.

1.3.3 WASTE PRODUCTS

No dumping petroleum products, anti-freeze, tires, etc. on track premises – any violation may result in fines, loss of points, and/or loss of monies. Any competitor that spills antifreeze, or any non-approved chemical, on the track or in the pit area, will be held responsible for all investigation and clean up costs.

1.4 DRIVERS

Drivers must be at least 14 years of age to drive in the 4 cylinder division.

Drivers must be at least 16 years of age to compete in all other divisions. Drivers younger than 16 wishing to compete in other divisions must be approved by track Officials.

Any driver under 18 years of age must have an insurance minor release form filled out, signed and on file with Impact Motorsports LLC before admission. Drivers must purchase and wear a wrist band for each division in which they are competing.

Drivers are considered independent contractors and assume responsibility for all charges, premiums and taxes, if any, payable on any funds received as a result of participation in any events. Impact Motorsports LLC and sponsors reserve the right to use photos of any driver or their car for promotional purposes.

Drivers must wear full face helmet and seatbelts whenever racecar is on the track. The only exception is during the National Anthem.

1.5 TRACK PACKING

All cars in all divisions are required to assist in packing the track when needed. When a division is called to pack the track, all cars in the pit area must report to the track. Any cars that do not assist in packing the track will start all races in the last row of their race for the entire Event. Alternate vehicles may be used for track packing, but must be registered with the Speedzone. One track packing vehicle per car. Packing vehicle must utilize the same tires as the division and display the car number. Track packing vehicles kept at the track must park in designated area and leave key. **ALL vehicles must be in running order!**

1.6 MUFFLERS

All cars must be equipped with a muffler. Cars cannot exceed a noise level decibel reading of 95 measured at 100 feet from the outside of track. Any car with noise levels that contribute to a violation of the community noise limit: loss of points and money for Event; and/or accumulated points for current year.

1.7 CAR NUMBERS

All numbers will be registered with Impact Motorsports. All numbers must have a letter. The letter must be at least A **6" OR TALLER**. All numbers on the doors must be a minimum of 20" tall and 3" wide. A **6" OR TALLER NUMBER REQUIRED ON FRONT AND BACK OF CAR**. Numbers must be in a contrasting color. No metallic numbers. Numbers must be kept clean and to be between 00 and 99. Failure to meet these requirements you risk the chance of not being scored or allowed to compete. A number is required on the roof, readable from the right side, and must be as large as possible. IT IS THE RESPONSIBILITY OF THE DRIVER TO ENSURE THAT THE SCORERS CAN READ THE CAR NUMBER. IF YOUR CAR NUMBER IS NOT READABLE FROM THE SCORING TOWER, THE CAR WILL NOT BE SCORED UNTIL THE NUMBER IS CORRECTED.

Track Officials reserve the right to assign or restrict the display of graphics and advertising on race cars. Offensive graphics or slogans are not permitted. All Competitors agree to accept Track's decision in this matter.

Where required, participating sponsors emblems, decals, or Track technical stickers, will be placed in the position designated by Track Officials. Cars that do not display all sponsors emblems, or decals, will receive less prize money.

1.8 BALLAST WEIGHT

Ballast weights must be painted white with car number clearly marked on it.

1.8.1 IF WEIGHTS FALL OFF CAR

a) FIRST OFFENSE:

Car will be black flagged for that event, plus a \$1.00 dollar per pound lost fine.

b) SECOND OFFENSE:

Car will be disqualified for that event plus a \$2.00 dollar per pound lost fine.

1.8.2 DRIVE SHAFT

Drive shaft must be painted white and car number on it.

1.9 DIAGRAMS

Diagrams published in this rule book are intended for reference guidelines only unless otherwise noted. The only exception to this rule will be in accordance of the Modified division. The Modified division will continue to follow the rules as written in this rule book.

1.10 DRIVERS MEETING

All drivers, or a representative from each car, must attend the drivers meeting. Attendance may be taken. If the driver, or car representative, is not present, the car will start all events in the rear that the car qualified for. Each car will be exempt from this rule its first night at the track to accommodate newcomers.

1.11 TRANSPONDERS

Transponders are required on all race cars. It is the driver's responsibility to make sure the transponder is in the correct placement and secured. If a transponder falls off during competition, the penalties are as follows:

a) First offense

Heat race - black flag and put in „B" feature.

Feature - black flag and scored in last place with that place points and money

b) Second offense

Heat race and/or feature - black flag and loss of points and money for night.

c) Third offense

Suspension and fine, discretion of track promoter and Track Officials.

If transponder stops working you will be scored until verification of missing or broken transponder is clarified. Practices will be used to test and record transponder locations. IT IS THE RESPONSIBILITY OF THE DRIVER TO ENSURE THAT THE TRANSPONDER IS FUNCTIONING PROPERLY. IF YOUR TRANSPONDER IS NOT FUNCTIONING PROPERLY, THE CAR WILL NOT BE SCORED UNTIL THE PROBLEM IS CORRECTED.

1.11.1 TRANSPONDER LOCATIONS

1.11.1.1 Modifieds, Grand Nationals, Street Stocks and Sport Modifieds must be placed twenty four inches (24"), or more, behind the center line of the rear axle to the forward edge of the transponder, on the right side frame rail.

1.11.1.2 Dirt Devils are to be located in the spare tire well area recommended 12", or more, behind the center line of the rear axle to the forward edge of the transponder.

Note: No metal of any kind can be between transponder and track. Transponder pouch should be placed horizontally with no obstructions between the pouch and the ground. If transponder pouch is mounted to round tube frame components with nylon ties, secure the transponder pouch so that it cannot spin from its horizontal position. This may be done by duct taping the nylon ties to the frame rail.

All transponder locations must have a clear view of track for signal to transmit.

1.12 PIT CLOSING TIME

The pit area will be closed at 12:00 am, (Midnight). Failure to clear the pit area within this period will result in the loss of prize money, and/or points. The amount of prize money, and/or points that will be lost is the decision of the Competition Director, and/or track management, which is final. All drivers are responsible for keeping their pit stalls clean. Track management reserves the option to assess pit stall cleanup costs for failure to keep pit stalls clean and use proper waste disposal containers.

SECTION – 2 SAFETY

2.1 TRACK SAFETY

Racing is an inherently dangerous sport. Each Competitor assumes the risk of injury or death when he/she participates in an Event. Competitors are solely, and directly, responsible for the safety of their race cars and racing equipment. OSHKOSH SPEEDZONE RACEWAY IS NOT RESPONSIBLE FOR THE ADEQUACY OF A COMPETITORS RACE CAR OR RACING EQUIPMENT. NO EXPRESS, OR IMPLIED, WARRANTY SHALL RESULT FROM THE PUBLICATION OF, OR COMPLIANCE, WITH THESE RULES. These rules govern the conduct of an Event, and, by participating, Competitors are deemed to be in compliance with these rules.

2.2 SAFETY EQUIPMENT

A full face, helmet and face shield, Snell-rated SA2000, SFI 31.1/2005, SA2005 or SA2010 required or manufactured build date for no longer than a 10 year time period and MUST be in good condition. Helmet and face shield must be worn at all times car is on the track and must accompany vehicle at time of inspection. Roll bar padding required in driver compartment (*Fire retardant recommended*).

2.2.1 REQUIRED

SFI-approved full fire suit, fire retardant gloves, shoes, driver-side window net, minimum 16 inch by 20 inch ribbon or mesh style, and must be mounted to roll cage so latch is at top front of window.

Seat belt and shoulder harness recommended being date stamped, 2010, 2011 or 2012 and being in good condition. (Y-type shoulder harness prohibited.) Belts must not be frayed. Seat belt and shoulder harness must be installed according to manufacturer's recommendations. Lap and shoulder belts must be approved snap-type release belts at least three (3) inches wide and securely fastened to the frame and or roll cage. A crotch or anti-submarine belt is mandatory, and must be a minimum two (2) inches wide.

2.2.2 RECOMMENDED

Neck brace or head and neck restraint. Right and left seat head supports required if using head restraint system with no neck collar. Fire retardant head sock and underwear; collapsible steering shaft recommended. . IF NO NECK BRACE IS USED A FIRE RETARDANT HEAD SOCK MUST BE USED!

2.3 DISCONNECT SWITCH

A kill switch required within easy reach of driver and must be clearly marked „OFF“ and „ON“.

A second battery disconnect switch, must be mounted behind or next to driver on deck cover and within reach of the safety crew. The disconnect switch must shut off all power to the car and be brightly colored. The battery must be securely installed. The positive terminal must be covered.

2.4 HEAD CLEARANCE

All divisions must adhere to the 2 inch head clearance rule!

Measurements will be taken from top of helmet to top of top roll bar with driver strapped in race ready!

SECTION - 3 PENALTIES

3.1 PROCEDURE

If a Raceway Technical Official observes, or is made aware of, a violation of the Raceway Rules, by a Competitor, Raceway Officials will investigate claim and can impose an appropriate penalty if found to be true.

3.2 EMERGENCY ACTION

If an act by a Competitor is determined by Raceway Officials, Raceway Technical Officials, or persons serving under Raceway direction, to threaten the orderly conduct of an Event, the Raceway Officials can take emergency action against the Competitor. Such action may include: arrest; ejection from the racing premises; suspension from competition; or any other action to remove the threat created by the Competitor. Examples of conduct warranting emergency action include, but are not limited to: consumption of alcoholic beverages in pit area before or during an Event; use of illegal drugs before or during an Event; harassment, verbal abuse, or assault of any Raceway Officials, Raceway Technical Official, or Competitor; fighting; reckless driving; and failure to obey a black flag or directions of a Raceway Official. The emergency action will remain in effect for the period of time determined by the Raceway Officials, except for an ejection which is final and non-appealable.

3.3 PAYMENT of FINES

Fines must be paid to Oshkosh Speedzone Raceway and will be deposited in the current discretionary fund. Failure to pay fines may result in suspension from competition. All unpaid fines may be collected, by Oshkosh Speedzone, by deducting the amount from the purse or point fund. If the Competitor is not a driver, the fine may be deducted from the purse or point fund of the driver with whom the competitor is associated at the time of the violation. Any unpaid fines remaining at the end of the racing season will be carried over to the next racing season and be deducted from the purse or point fund until all monies are collected.

3.4 SCOPE of PENALTIES

Penalties for violation of Raceway Rules are determined by the severity of the violation. Penalties include, but are not limited to: fines; loss of points; loss of purse; disqualification; suspension of driving privileges. Oshkosh Speedzone Raceway will use the following guidelines for determining penalties. **A greater or lesser penalty may be imposed depending on the circumstances.**

3.5 GENERAL

- a) Any Competitor who performs an act or participates in an act deemed by Track Officials as detrimental to auto racing or the Track: a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- b) Any Competitor who signs the release sheet for anyone else: ejection.
- c) Any parent or legal guardian that falsifies an insurance waiver for a minor Competitor: ejection of the minor Competitor; and/or suspension of the minor Competitor; and/or loss of points and money for Event; and/or loss of accumulated points for current year; and a fine determined by Track Officials.
- d) Any Competitor who harasses, verbally abuses, or assaults any Track Official, Technical Official, or persons serving under Track direction: ejection; a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- e) Any Competitor who participates in fights on the racing premises: ejection; a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- f) Any Competitor who, while participating in a Speedzone Event, consumes any alcoholic beverages and/or illegal drugs, or is under the influence of alcohol and/or illegal drugs: a fine determined by Track Officials; ejection & disqualification; and suspension for a period of time determined by Track Officials.
- g) Any Competitor who stops on the track to argue with a Track Official: a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- h) Any Competitor who fails to correct an unreadable car number: stop scoring of the car until the number is corrected; and/or a fine determined by Track Officials.
- i) Any driver not wearing a full driver's suit and/or gloves and/or other mandated driver safety equipment during an event: "disciplinary" black flag; and/or a fine of \$25.00.
- j) Any Competitor who leaves tires in the pit area or anywhere on the track property: a fine of \$25.00/tire.

3.6 ROUGH DRIVING

- a) Disciplinary black flag, fines, suspensions and loss of points based on severity of incident is at the discretion of track officials.
- b) Intentional use of vehicle in a malicious manner - minimum \$1000 fine and/or suspension is at the discretion of track officials.

3.7 FUEL

Sample may be taken from any car at anytime.

- a) First violation - \$250 fine.
- b) Second violation - 3 event suspension plus the cost of inspection fees. Only completed Events are counted towards suspension.

SECTION - 4

TRACK PROCEDURES

4.1 RACE DECISIONS

All decisions by Oshkosh Speedzone Raceway Officials involving track procedures are final, and non-appealable.

4.2 RAIN OUTS

Raceway Officials will consult with the track owner to determine if the Event will be postponed.

In the event of a rain out prior to the opening of the pit gate, information can be obtained by calling the race track. The track phone number for "Oshkosh Speedzone Raceway" is **(920) 252-1463 / (920) 279-9096**.

In the event of a rain out after the pit gates are open, all qualifying races and B Main races must be completed in order to receive points and purse. Completed races will receive full points and purse. Uncompleted A Main races will receive total points and purse for the race, divided equally among the race cars. Rain out passes, if issued, or wrist bands from the event, are good only for the next two (2) completed events at the track where the rainout occurred.

4.3 WEIGH IN

All drivers are encouraged to weigh their race car, prior to racing. Any race car under the allowable weight is subject to disciplinary action. Penalties will be imposed in accordance with **SECTION 3 - PENALTIES**. Random weight checks may be conducted at any time. Weights will be determined by the track scale, which is considered official. The top five race finishers in all divisions, or as designated, must report to the Claim/Inspection Area immediately after the race. Winners may go into victory lane without penalty then to scaling.

4.4 LINEUPS

It is the driver's responsibility to make sure your car is **REGISTERED AND SIGNED IN** and car number is on the lineup sheet. Failure to do so will result in being placed at the rear of the heat lineups.

All cars must be lined up in the staging area in the proper position, when the front row cars begin to move out of the staging area. Cars entering the staging area or race field after the front row cars begin to move must start the race at the rear in the order they join the field. Exceptions will be made for drivers competing in more than one division per night, or a Competitor who notifies a pit steward of extenuating circumstances.

4.5 CAR AND DRIVER CHANGES

A driver may drive a different car that has been qualified but will start the event from the rear. If a driver qualifies for the feature but needs to use an unqualified car to compete, the driver must race through the "B" main to qualify for the feature. The driver will start in the last place position of the "B" main.

ALL DRIVER AND CAR CHANGES MUST NOTIFY ONLY THE COMPETITION DIRECTOR, COMPETITION DIRECTOR WILL NOTIFY SCORING TOWER OF CHANGES!

4.5.1 DRIVER SUBSTITUTION

A driver may have a substitute driver once a year. Original driver must notify registration crew one week in advance. Registration crew will make note of the change.

4.6 CONDUCT

The driver is the sole spokesperson for the car in any and all matters pertaining to an Event, and is responsible for the actions of the pit crew, and owner. Determination of crew affiliation will not be limited to the pit sign in sheet. It will be a determination made by the track staff. Harassment or abuse of Raceway Officials will not be tolerated and is subject to disciplinary action.

4.7 INJURIES

All injuries must be reported to a Raceway Official prior to leaving the race premises. Track insurance will not cover any unreported injuries. Any driver involved in a roll over, or severe accident, must be checked and cleared by the safety staff prior to returning to the track.

4.8 CAMERAS

In car cameras will be allowed for personal use only and are thus deemed unofficial. Unofficial cameras will not be used to change or determine any calls made before, during, or after a race event.

4.9 SAFETY VEHICLES

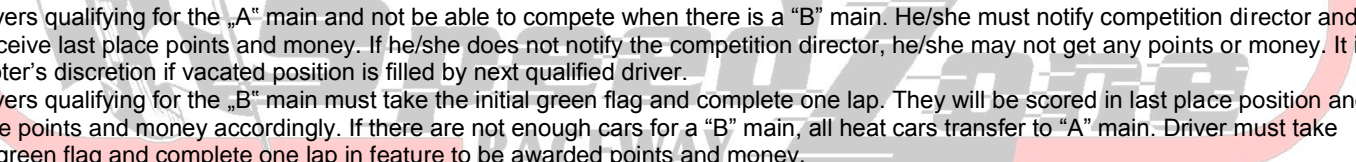
Any driver that hits any track vehicle will pay for damages!

SECTION - 5 POINTS/PAYOUT PROCEDURES

5.1 POINTS

Weekly „A“ feature win is 40 points with each subsequent position worth one point less, with 24th position in feature worth 17 points. If track runs a „B“ feature(s), first non-transferring driver receives 16 points and each subsequent position, back to sixth position, receives one less point per position. Drivers that do not qualify for „A“ or „B“ feature will not receive points or money. Heat points are awarded. 1st - 3 points; 2nd - 2 points; 3rd - 1 point.

1. 40	Transfer	Heat 1 - 3
2. 39	Transfer	Heat 2 - 2
3. 38	Transfer	Heat 3 - 1
4. 37	Transfer	
5. 36	16	
6. 35	15	
7. 34	14	
8. 33	13	
9. 32	12	
10. 31	11	
11. 30	11	
12. 29	11	
13. 28	11	
14. 27	11	
15. 26	11	
16. 25		
17. 24		
18. 23		
19. 22		
20. 21		

- 
- a) Drivers qualifying for the „A“ main and not be able to compete when there is a „B“ main. He/she must notify competition director and will receive last place points and money. If he/she does not notify the competition director, he/she may not get any points or money. It is promoter's discretion if vacated position is filled by next qualified driver.
 - b) Drivers qualifying for the „B“ main must take the initial green flag and complete one lap. They will be scored in last place position and receive points and money accordingly. If there are not enough cars for a „B“ main, all heat cars transfer to „A“ main. Driver must take initial green flag and complete one lap in feature to be awarded points and money.
 - c) Points remain with the driver, not with the car.
 - d) Divisions that have less than ten entries, one point fewer than normal is awarded for each car less than ten. For example, if nine cars entered, feature win would then be worth 39 points; for eight entries, win would be worth 38 points, etc.
 - e) Points will determine track championships. In case of a tie, feature wins will be counted, then seconds, thirds, etc.

5.2 PAYOUT

All positions in features get money and/or a pass. „B“ mains receive money if the initial green flag and one lap is taken. Heat races do not pay – they are for points and qualifying for „B“ and „A“ features. Earnings are paid out at the following event from 4:00 to 6:00 in pole shed. Any unclaimed earnings will be available the following week during the same time period. Any driver wishing his check mailed must provide a stamped self addressed envelope (SSAE). Any money not claimed after **30** days will be returned to Impact Motorsports LLC.

Driver must compete in seventy percent (70%) of scheduled events and attend awards banquet to be eligible for point fund.

5.2 LINE-UP PROCEDURES

Heat races will be lined up according to the starting position drawn by the driver, for the first Event only. After the first Event, qualifying races will be lined up according to average Speedzone driver points, with the lowest point average drivers to the front and the highest point average drivers to the rear. Drivers without Track points will be placed in the last row. If a driver is absent for two consecutive events (excluding rainouts), they will be treated as a new driver the first week they return. The deadline for race car registration and lineup is 6:00 PM, or you will be considered late and placed in the last row.

Point averages are = (total points) / (number of events competed in).

For the first night of racing all drivers draw two numbers. The first number is for heat starting position. The second number is the redraw for „A” feature starting positions. One heat - 5 redraw; two heats - 10 redraw; three, four, five, or six heats - 12 redraw.

„B” features are lined up from heats with highest finishers at the front.

‘A’ features: Heat qualifiers are lined up by inverted point average. When one heat is run the top 5 qualifiers from the heat are inverted. When two heats are run the top 10 qualifiers will be inverted. When three, four, five, or six heats are run the top 12 qualifiers will be inverted. The remaining heat qualifiers are lined up behind inverted field as they finished. A driver that qualifies for the invert but has no point average shall be lined up in the last invert position. Should there be more than one driver with no point average; they will be lined up at discretion of officials. Impact Motorsports LLC also reserves the right to relocate a driver to the rear of the field if deemed necessary for the safety and well-being of all involved. Remainder of „A” feature is lined straight up from heats and/or „B” features. If a driver is absent for two consecutive completed events they are treated as a new car (zero point average) for the first week he/she returns. Total points are not lost.

5.4 LAPS

Heats - 1 lap per car with a maximum of 10 laps, or 1 minute per lap.

„B” features will normally be 1 lap per car with a maximum of 15 laps, or 1 minute per lap.

„A” features will normally be 20 laps.

Track officials have the option to alter race length based on car count, weather, time, etc. Race will be scored complete after laps are completed or time limit is up.

5.5 RACING

Cars must enter the track from turn three and leave the track using the turn two exit.

On original starts the front two cars are double file and pace the start together with moderate speed. These front row cars are given 2 chances at a clean start before both cars being sent to the rear of the starting field. No jackrabbit starts allowed – flagman’s discretion. Any car(s) causing a yellow flag before the initial start of the race will restart the race in the last row.

All restarts will be double file, except for the leader who will be alone in the first row. The car leading the race controls the restart. A restart cone will be placed at the exit of Turn 4. After the “one lap to go” signal is given, and/or the drivers are notified over the Raceciever, all cars must stay nose to tail. Lagging back is prohibited. On all restarts, all cars must maintain their position and may not accelerate until the lead car passes the restart cone. Any cars lagging back prior to the restart, hitting the cone, or passing to the left of the restart cone, will be penalized two positions on the next caution. Any cars passing prior to the lead car passing the restart cone will be penalized two positions per position advanced on the next caution. If no caution occurs, the driver will be penalized at the end of the race. Any car(s) causing a yellow flag before the restart of the race will restart the race in the last row. Single file restarts will be at the discretion of the flagman.

Any single driver spinning out unassisted and causing one (1) yellow flag in a heat race, or two (2) yellow flags in the „B” feature or „A” feature race, will receive a disciplinary black flag. This does not apply to a driver that spins out to avoid a wreck. A driver that spins out to avoid a wreck and does not make contact with the wall or any other vehicle may receive their position back. The determination of whether a driver spins to avoid a wreck is a judgment call by the flagman, which is final.

Driver must use pit lane if he/she wishes to re-enter the race. Any car on pit lane will rejoin the field from the rear. Driver must pull over to the right as much as possible while on pit lane. If a car pulls off the track in turn three the car is considered in the pit and out of the race.

If a driver enters pit lane for repairs, repaired part must be checked by pit lane person staged at the end of the pit lane. If driver fails to stop, he/she will be black flagged and not allowed to re-enter event. Lapped cars will start race at back of field.

No one but the driver is allowed on the track at any time during an event. Driver will be disqualified from the event if pit crews enter the track unless specified by track officials. Driver will lose all points and pay for that event.

No unapproved communication, recording, or traction control devices will be allowed in any car.

A SW1600 model Raceciever, tuned to channel 0000 (454.000) is mandatory for all divisions.

5.6 STARTS AND RESTARTS

On ALL restarts, drivers should line up single file front to back on caution laps to give scoring time to put all cars in correct position.

Once positions are correct, drivers will be notified via Receiver to form two wide with leader in front by him/herself. On all new starts, two wide front to back. In the event the two front cars cannot make pace, at the flag mans discretion, the two cars may be sent to the back of the field and the row will just move ahead. On all starts and restarts, at the flag mans discretion, if the start cannot be accomplished, he may make a single file start. We will use the acceleration cones in the apex of corner 3 & 4 with the official starting cone at the end of corner 4.

5.7 FLAGS

5.7.1 GREEN FLAG

The green flag, and/or green light, indicates the start of the race or the restart of the race after a caution period. The green flag and green light will be displayed as the lead car(s) pass by the flagman. All race starts will be double file, nose to tail. ("Nose to tail" means staying in the tire tracks of the car ahead of you.)

5.7.2 WHITE FLAG

The white flag indicates that there is one lap remaining. If a yellow flag occurs after the white flag is displayed and the leader has crossed the start/finish line, the race is considered complete. All cars that have passed the start/finish line will be scored as they crossed the start/finish line. All remaining cars, **except** those involved in the incident, will be scored according to their last completed lap. The cars involved in the incident will be scored at the end of their last completed lap.

5.7.3 CHECKERED FLAG

The checkered flag indicates that the race is complete. Once the checkered flag is displayed to the race leader, the race is considered complete. All cars receiving this flag must slow to a reasonable speed and, with the exception of the winner, return to the pit area. Failure to slow to a "reasonable speed" may result in disqualification. The determination of reasonable speed is a judgment call by Track Officials, which is final. If the yellow flag occurs after the checkered flag is displayed to the race leader, all cars that have passed the start/finish line will be scored as they crossed the start/finish line. All remaining cars, **except** those involved in the incident, will be scored according to their last completed lap. The cars involved in the incident will be scored at the end of their last completed lap. Race winners will report to the "winner's circle" for post race ceremonies. Finishing positions will be determined by the number of laps completed, whether the car is running, or not. Effective Date: January 1, 2010

5.7.4 YELLOW FLAG

The yellow flag, and/or yellow light, indicates caution on the track. All cars receiving this flag, and/or light, must slow, hold their position, and form a single file line behind the lead car. **Absolutely no racing back to the yellow flag.** The penalty for racing back to the yellow flag, as determined by the flagman, is restarting the race at the rear. If a pace vehicle is used, the leader must line up behind it. Cars will be lined up as they were scored on the last completed lap. Lapped cars will be placed in the back of the field. Any cars entering pit lane during the caution period must restart the race at the rear, in the order they return to the track. Re-entry under the yellow flag is not permitted until the lineup is complete. If the yellow flag occurs before the completion of one lap, the cars will be lined up in their original starting order **except** those cars involved in the incident, which must restart the race at the rear. Any driver, as determined by the flagman, intentionally causing a yellow flag (such as spinning out another car, stopping on the track, or capable of resuming the race after a spin but does not) will receive either a one lap penalty or "disciplinary" black flag. No car may pass the pace vehicle unless directed by a Raceway Official. Any car illegally passing the pace vehicle is subject to the black flag. Prior to restarting the race, the flagman will signal one lap to go.

5.7.5 BLUE FLAG WITH DIAGONAL YELLOW STRIPE (FASTER TRAFFIC APPROACHING)

The blue flag with the diagonal yellow stripe indicates that faster traffic is approaching. Cars receiving this flag must prepare to yield to faster traffic. Cars should hold their line on the track and allow the faster traffic to pass. If the slower cars are running side by side, they must form a single line at the bottom of the race track. Failure to obey a "faster traffic approaching" flag is subject to a "disciplinary" black flag.

5.7.6 RED FLAG

The red flag, and/or red light, indicates that the race must stop immediately, regardless of the position of the cars on the track. During a red flag, pit crew members are not allowed on the track. All car repairs must be done on pit lane. If the red flag occurs before the completion of one lap, the cars will be lined up in their original starting order **except** those cars involved in the incident, which must restart the race at the rear. If the red flag occurs after one complete lap, the cars will be lined up in single or double file according to the last completed lap. Prior to restarting the race, the flagman will signal one lap to go.

5.7.7 BLACK FLAG

Pull off the track in turn two onto pit lane and Pit Steward will advise you the reason for the flag. If black flag is for a mechanical problem, car may return to track after making necessary repairs. If black flag is for disciplinary reasons, the car and driver are disqualified from race.

SECTION - 6 INSPECTIONS

6.1 INSPECTIONS / ENGINE EXCHANGE / PURCHASE / PROTEST / SEALING

Track officials have the right to inspect any engine or car at any time.

The Competitor will be given a "reasonable amount of time" to remove components from the car for post race inspection or claim. The Competitor will be notified as to what is considered a "reasonable amount of time" at the beginning of the inspection or claim. If the removal of the components is not completed in this time period, the car will be disqualified.

The cars crew and or driver will perform the actual teardown or engine removal with their own tools and equipment. Only two (2) of the cars crew and the driver will be allowed in the Inspection/Claim area.

The individual(s) representing the car during post race inspection or claim may not consume any alcoholic beverages, or be under the influence of alcohol, until the post race inspection of the car or claim is complete.

The top five race finishers in all divisions, or as designated, must report to the Inspection/Claim Area immediately after the race.

6.2 ALL CLAIMS, EXCHANGES AND PROTESTS WILL BE SUBJECT TO A \$50.00 ADMINISTRATION FEE.

6.2.1 MODIFIEDS

ENGINE CLAIMING RULES:

6.2.1.1 \$1,050 cash, or \$100 and exchange, claim on engine, flywheel and balancing plates (\$25 goes to wrecker and \$25 to official for each engine). Claimed driver has option of accepting cash, or exchanging engines with claiming driver.

6.2.1.2 Claim does not include - 1. Clutch, 2. Pressure plate, 3. Bellhousing, 4. Headers, 5. Carburetor, 6. Starter, 7. Motor mounts, 8. Oil/temp. Sending units, 9. Carburetor spacer, 10. Fan and pulleys, 11. Clutch ball, 12. Clutch arm, 13. Throw out bearing, 14. Dip stick, 15. Water pump, 16. Fuel pump, rod and plate, 17. Distributor, 18. Plug wires, 19. Water outlet and restrictor, 20. Breathers.

6.2.2 STREET STOCKS

Track officials have the right to purchase an engine for \$750.

Protest only, \$200 top, \$300 bottom, \$500 for complete engine.

6.2.3 GRAND NATIONALS

Protest only, \$200 top, \$300 bottom, \$500 for complete engine.

6.2.4 DIRT DEVILS

\$600 claim swap for entire car except for the seat, seat belts, window net and fuel cell (if equipped). Track officials reserve the right to buy any car for **\$1000** less the seat, seat belts, window net and fuel cell (if equipped).

6.2.5 SPORT MOD

ENGINE CLAIMING RULES

6.2.5.1 \$550 cash claim on engine, \$25 goes to wrecker for pulling engine and \$25 goes to official.

6.2.5.2 Claim does not include - 1. flywheel, 2. clutch, 3. pressure plate, 4. bellhousing, 5. breathers, 6. carburetor, 7. starter, 8. motor mounts, 9. oil/temp. sending units, 10. fan and pulleys, 11. clutch ball, 12. clutch arm, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor, 18. plug wires, 19. water outlet and restrictor, 20. headers.

NOTE: During engine claim, block and head numbers should be verified, heads, intake, connecting rods and crankshaft should be visually inspected and one spark plug must be removed to check flat top or dish pistons prior to transfer of engine to claiming driver. If, at this time, engine is found illegal, claimed driver is disqualified and suspended from all events until such time as a \$250 fine is paid, for first offense. Claimer then has option to accept or decline engine - if declined, driver is not charged with claim. Second offense penalties will be \$500 fine and/or suspension.

6.3 WHO CAN BE CLAIMED/PROTESTED

Top four finishers of the „A“ Feature whether running or not are subject to engine exchange claim / protested. The top four drivers must proceed directly to the claim / protest area. Driver must enter the claim / protest area. Drivers must remain in the claim / protest area until directed otherwise by a track official. If any of the top four finishers do not go directly to the claim / protest area, they will be treated as refusing to exchange / protest.

6.4 WHO CAN CLAIM/PROTEST

Any driver finishing fifth or lower on the lead lap of his/her third consecutive feature may make a claim / protest. Dirt Devils - driver must finish within five positions of the car he/she is claiming and be on the same lap.

Drivers changing cars or provisional drivers can not claim. Pill claim will be enforced the last four points nights of the season and/or as needed. Pill claim means the claimer draws at random from the top four positions to determine the car to claim. In case of multiple claims for the same car, the lowest qualified driver gets priority.

Any driver making a claim / protest must drive his/her racecar, under it's own power, directly to the claim / protest area. Driver can not stop his/her vehicle or consult with anyone else. Claims / protests must be made to a track official with cash upon declaration of intention to claim / protest. Only the driver may request, agree to, or refuse claim / protest and is the only one allowed in the claim / protest area. Driver making claim must complete claim / protest or lose all money and points for the night. Once claim is accepted, claimer and claimer may not restart their engines. Cars will be pushed to the removal area.

First acceptance or refusal by driver being claimed / protested is binding.

6.5 FIRST REFUSAL PENALTIES

Forfeiture of all earnings for the night and loss of points for the year. Driver also forfeits his/her right to claim / tear down for 12 months from date of refusal.

Driver is suspended for 4 events from date of refusal.

6.6 SECOND REFUSAL TO EXCHANGE

Result in the driver being suspended from competition for one year to date.

Any driver found to be claiming / tearing down for someone else will lose all points for the year and be suspended for a minimum of two completed races.

All claimed engines must be removed from the car at the track. Once claim has been accepted the exchange is final.

There is a 1½ hour time limit on engine removal, if not complete in this time, it will be considered a refusal of claim.

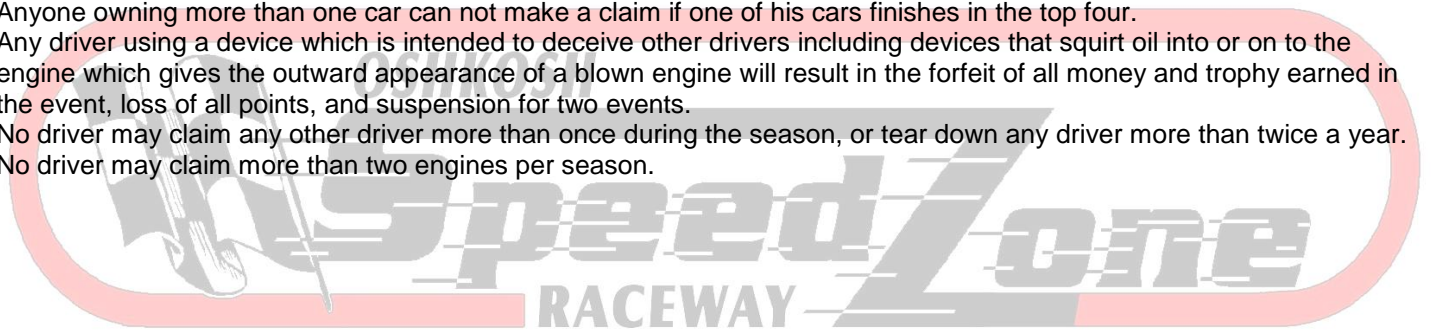
Any sabotage to a claimed engine will result in saboteur's suspension from competition for one year to date.

Anyone owning more than one car can not make a claim if one of his cars finishes in the top four.

Any driver using a device which is intended to deceive other drivers including devices that squirt oil into or on to the engine which gives the outward appearance of a blown engine will result in the forfeit of all money and trophy earned in the event, loss of all points, and suspension for two events.

No driver may claim any other driver more than once during the season, or tear down any driver more than twice a year.

No driver may claim more than two engines per season.



SECTION - 8 GRAND NATIONAL DIVISION 2012

ALL GENERAL TRACK RULES APPLY. PLEASE REFER TO GENERAL TRACK RULES SECTION FOR CAR NUMBER REQUIREMENTS, APPEARANCE GUIDELINES, START TIMES, AGE REQUIREMENTS AND MANY MORE ISSUES. RACE TEAMS NOT COMPLYING WITH GENERAL RULES WILL NOT BE ALLOWED TO COMPETE. RULE VIOLATIONS ARE LISTED IN THE GENERAL RULES.

SEE TRACK OPERATIONAL RULES FOR SAFETY RULES, SEAT BELTS RULES

IF IT DOES NOT SAY IT IS ALLOWED IN THE RULES, IT IS ILLEGAL. IF YOU ARE UNSURE OF ITS LEGALITY PLEASE CONTACT A SPEEDZONE OFFICIAL.

THANK YOU!

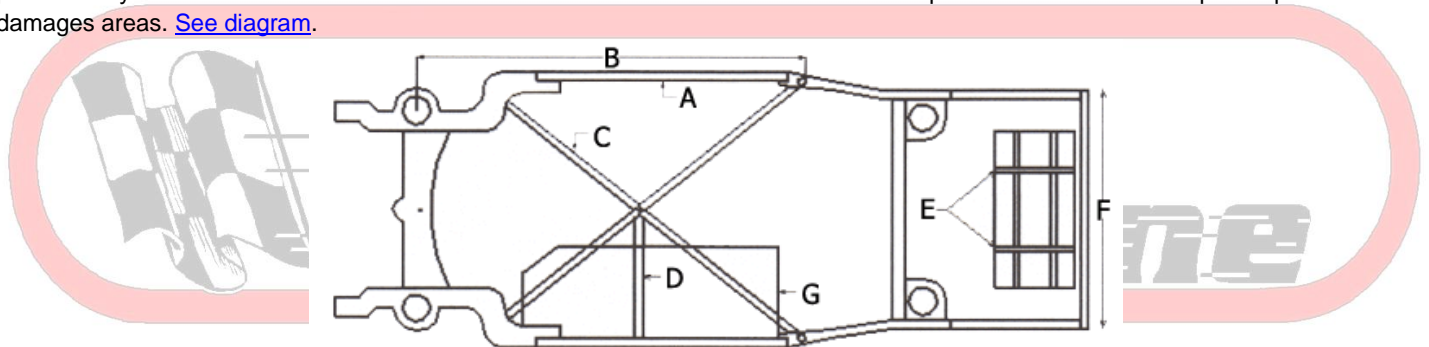
WEIGHT RULE – 3000 LB.

8.1 Eligible Models

GM, AMC, Chrysler, Ford or U.S. manufactured auto. No pickups, station wagons or convertibles allowed Special built models accepted upon official approval

8.2 Frame

1978 to 1988 GM metric frames only unless run previously. Frames must be X-ed; side rails must be plated where open. Front cross member may be notched and plated for fuel pump clearance only. Weight jacks or screw jacks allowed. Frame rails behind rear spring pockets may be cut and fabricated. Must have a minimum 108" wheelbase. No other improvements allowed except to repair rusted or damaged areas. [See diagram.](#)

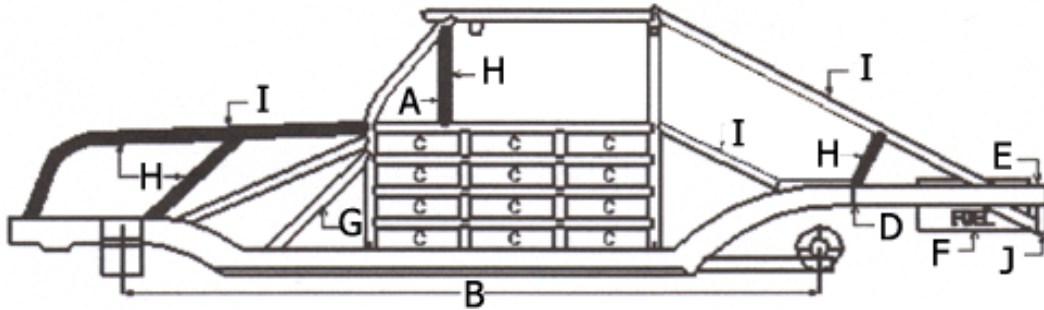


8.2.1 Construction Guidelines

- A. A. Frame plating material (4" x 1/8") flat strip steel.
- B. B. Roll cage main hoop set back 82 1/2", measured from center of lower ball joint to back of main hoop tubing. No portion of the seat may extend beyond 80 3/4" from the left lower ball joint
- C. **Mandatory** X-brace, (Choose 1 material, no mixing).
- D. D. Drivers compartment safety bar (same material as X-brace).
- E. E. Fuel cell safety straps (minimum 4) 1" x 1/8" strap steel.
- F. F. Tail section replacement, same dimensions as stock frame.
- G. G. Floor plate 1/8" steel plate, minimum width 20" at seat.
(X-brace must tie into original frame, not frame plating)

8.3 Roll Cage

Four-point roll cage constructed of 1 3/4" x .095" wall steel round tubing. NO Layback cages. Roll cage main hoop must be at a 90 degree angle to main frame rails at the discretion of safety inspector. One front and one rear cross brace, recommended height at 20". The main hoop behind driver must be cross-braced. Two passenger side and four driver side door bars, with driver side door bars plated and having two upright bars between each driver door bar. Wing window bars on drivers side mandatory; passenger side recommended. Foot protection bar (may be plated for added safety). Two front support bars must start at cross brace height at roll cage corners and extend forward to frame. Four rear support bars must extend to frame, two starting at the rear cross brace at roll cage corners and two from the top corners of the rear cage hoop. **Halo bar must have one cross bar running either front to back or diagonal made of 1-3/4" x .095" wall steel round tubing.** [See diagram.](#)

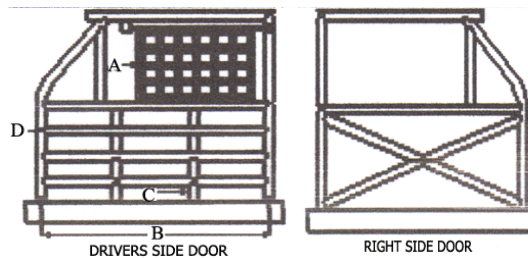


8.3.1 Construction Guidelines

- A. A. Wing window bars, **mandatory Drivers side opening must be a minimum of 15 inches!**
- B. B. Minimum wheelbase 108"
- C. C. **Mandatory** safety plating, 1/8" steel minimum
- D. D. Stock frame from this point forward
- E. E. Rear frame tail section, 2' x 3' x .095 steel tubing
- F. F. Fuel cell can mandatory (20 gauge steel)
- G. G. Foot area protection bar, **mandatory**
- H. H. Dark shaded bars may be 1 1/2" x .065 wall steel tubing, all other tubing 1 3/4" x .095
- I. I. All cages must have these front and rear support hoops/bars
- J. J. Braced .095 wall steel tubing, fuel cell guard

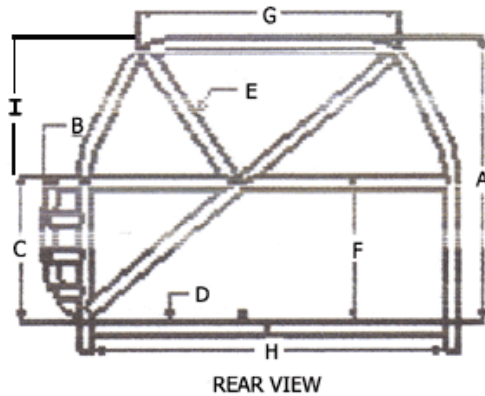
8.4 Drivers Compartment

High back aluminum racing seat is mandatory. Seat must be bolted in with six 3/8" Grade 5 or Grade 8 bolts. Four bolts in the seat bottom and two in the seat back, NO CARRAGE BOLTS! NO portion of the seat may extend 80 3/4" from the left lower ball joint. A window net with buckle release mounted on the top left corner is required. All roll cages must provide a minimum of 2" of clearance measured from the top of the roll cages to the top of the driver's helmet when driver is seated and strapped in driving position. The center of the steering wheel must be padded and all roll bars that the driver could contact must be adequately padded.



Construction Guidelines

- A. A. Approved window net, **mandatory**
- B. B. Recommended door bar length, 44"
- C. C. 2 door bar uprights between each door bar, **mandatory**
- D. D. 4 door bars evenly spaced on drivers side, **mandatory**



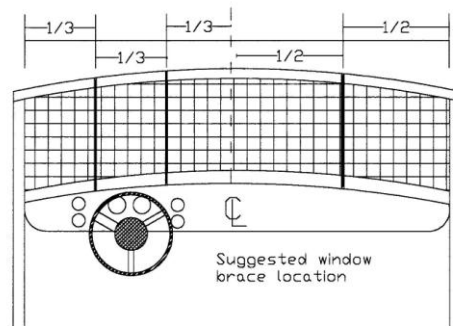
Construction Guidelines

- A. Main hoop (vertical to frame) recommended 39" high from top of frame
- B. Door bars should be flush to panel (with the body centered & mounted in stock location on the frame).
- C. Door bar height (minimum 20" high from top of frame)
- D. Floor pan (minimum 20" in drivers seat area).
- E. Extra main hoop, headrest bar (optional)
- F. Cross bar recommended height 20" (for shoulder harness)
- G. Roof hoop to fit roof side to side (minimum length 34")
- H. Roll cage width (same as stock frame width)
- I. Window opening (Minimum height 15")

8.5 Bodies

8.5.1 Production passenger car, stock or after market template/straight-up style steel or Aluminum body panels for fenders, quarter panels and trunk. **Plastic front fenders allowed.** NO Wedge or Late Model type bodies. Hood must be steel, aluminum or fiberglass and the back of the hood must be sealed down completely. Absolutely no air induction of any type allowed. **YOU MUST HAVE HOOD TO START RACE.** Door panels must be stock, sheet metal or aluminum. No door handles, locks, moldings, factory plastic or chrome accent pieces left on the stock panels. No fins, lips, rudders or other aerodynamic devices allowed. Factory or aftermarket plastic noses are legal. Rear of car must be completely enclosed, fender-to-fender from the trunk lid down. With a cover that is a minimum of 20" high. Nose of car must look stock appearing and neat looking – no wedge type or late model noses. Flat roofs prohibited, roof must retain stock contours. No holes in hood or hood scoops. Hood pins to be used in hood and trunk. Stainless steel windshield screen must be no less than .063" for mesh larger than 1/2" or no less than .035" for 1/2" or smaller mesh (no larger than 1" mesh screen allowed), to cover entire opening with three upright braces tip to bottom of 3/8" or larger solid rod. . [See diagram.](#)

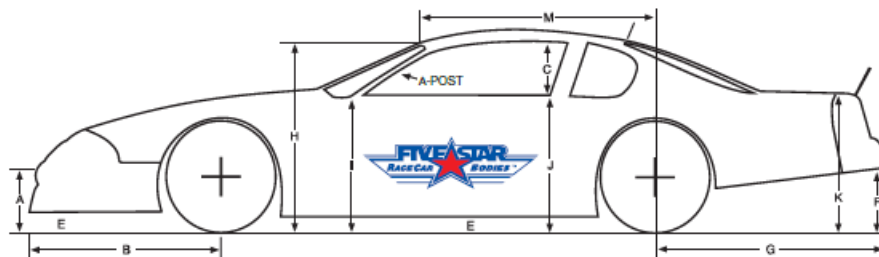
Spoiler allowed on rear of trunk lid not to exceed **6" in surface height. Rear spoiler to be 60 1/2" wide at widest point, no sideboards or forward braces. No rear window for safety reasons.** Two and one half inch (2 1/2) non-functional hood scoop allowed, no holes in hood or hood scoop allowed. **YOUR CAR MUST LOOK STOCK TO RACE!**



8.5.2 Body parts must be neat and well painted. Car numbers must be painted a contrasting color. Numbers must be painted on both sides and roof to face grandstand and numbers between 00 and 99. Numbers must be at least **18" high and 2" wide**. **Numbers of 6" or higher must be placed on the front and rear of the car to aid in line-ups.**

8.5.3 The car at its widest point, including rub rails, may not exceed 78"

8.5.4 Interior sheet metals to cover all openings, front, rear and side. Tapered floor from driver to right side can be any style as long as no higher than cross brace. Floor plate under driver of 1/8" steel, 20" wide and length of driver



GUIDELINE DIMENSION (INCHES)

A	NOSE HEIGHT ①	19 ¼
B	FRONT OVERHANG (MAXIMUM, ON CENTERLINE)	46
C	SIDE WINDOW OPENING (MINIMUM)	15 ¼
E	SIDE PANEL CLEARANCE (TYPICAL)	8
F	BUMPER COVER HEIGHT (MAXIMUM)	16
G	REAR OVERHANG ②	47
H	ROOF HEIGHT (MINIMUM) ③	51
I	FENDER HEIGHT (AT REAR)	35
J	DOOR HEIGHT (AT REAR)	35 ¼
K	QUARTER PANEL HEIGHT (AT REAR)	35 ½
M	FRONT OF ROOF TO CENTER OF REAR AXLE	60 ½

- ① Measured from ground to top of bumper line
- ② Base of spoiler at centerline to axle centerline
- ③ Measured 10 inches back from windshield (on centerline)

8.6 Engines

Sealed engines are only honored at track they are sealed at and may be required to be unsealed for inspection at other tracks.

8.6.1 American made production V-8 engine blocks only. Must be production cast iron block with standard external measurements in all respects with the exception of the permissible .060 over bore maximum. No aluminum, Bowtie, Rocket, SVO, 'X', or 'R' blocks allowed. No removing of **casting** numbers. No grinding or lightening.

8.6.2 Aftermarket crankshafts allowed and cannot weigh less than 48 pounds. Must have stock stroke for the block being used, and must remain unaltered except for the balancing. No lightweight, life edge, undercut counter weights or scalping allowed.

8.6.3 Rods

8.6.3.1 Forged magnetic steel connecting rods only. Rods must remain OEM stock length for the block being used. Maximum length of rod to be 5.7, Chevy 400 may use 5.7 rod. H-Beam rods allowed NO titanium or aluminum rods. Through bolts or caps screws allowed.

8.6.4 Pistons

8.6.4.1 Any flat top or reverse dome piston may be used as long as it has the stock wrist pin height for the engine being used (except 400 Chevy). Valve relief may be cut into the piston. No portion of the piston may protrude above the top of the block.

8.6.5 Camshaft

8.6.5.1 Any Hydraulic or flat tappet steel camshaft with a maximum of .500 lift measured at the retainer. Stock diameter tappets only. No mushroom or roller tappets allowed. No stud girdles or rev kits. Standard timing chain only - no gear drive or belt drives will be allowed. **Roller rockers and roller tipped rockers allowed. No shaft rockers.**

8.6.6 Cylinder Heads

8.6.6.1 **Only stock, cast iron head or World Products SR Torque #4266B, #4267B, 4360 or 4361. No Bowtie, Vortec, angle plug, SVO, W2, GT 40 or fuel injection type cylinder heads allowed.** Only two valves and one spark plug per cylinder. No titanium parts in the valve train allowed. Intake and exhaust ports must be in the original "as cast" configuration. Any evidence of any sanding, polishing, relieving, grinding, porting, chemical treating, abrasive blasting, alteration to the original form or the addition of material to the ports or combustion chambers is strictly forbidden. No external sanding, grinding or removal of **casting** numbers or symbols. Valves must be stock production size and in stock location. Small block Chevy maximum valve size is 2.02" intake and 1.60" exhaust, measured across the face of the valve. Stock production valve spring diameter for heads used only, no beehive springs allowed. Screw in studs and guide plates allowed. **Minor relieving for push rod clearance allowed.** No chemical or ceramic work allowed in internal areas of intake or heads.

Optional Engine Configuration (141 Only)

a. 062 & 906 Vortec Heads are allowed (valve size must remain stock) must be run with Edelbrock 7116 Alum intake & Holly 4412 Carb

8.6.7 Oil System

8.6.7.1 Oil pump must remain in stock location. No dry sumps or external oil pumps allowed. After market oil pans and breathers are allowed. Oil cooler, if used, to be in engine compartment only. Oil accumulators (Accusump's) may not be located in the drivers' compartment. 1" (one inch) inspection hole in oil pan is recommended for viewing crankshaft markings.

8.6.8 Intakes

8.6.8.1 Stock production cast iron intakes on engines over 365 cubic inch. Following aluminum intakes numbers allowed on 365 cubic inch or less - GM: Victor Jr. 2975 or 2972 and 2101, Weiland 7546 or 7547. Ford: 2981 or 2760 and 2665 or 2750, Cleveland 7516 or 7517. AMC: 2930 or 2035. Chrysler: 2915 or 2176. Edelbrock: 7101 and 7501. After market or stock intakes to remain untouched. NO Marine type intakes.

8.6.9 Exhaust

8.6.9.1 Stock cast iron exhausts manifolds or headers okay. No 180-degree headers or step headers. Exhaust must exit under car, behind driver and be angled down. Mufflers are mandatory and car must pass noise level test (95 decibel at 100 feet outside of track).

8.6.10 Ignition

8.6.10.1 Factory type ignition, no Multi-spark discharge or mags. Rev limiters allowed one chip system only. HEI type distributors must have the coil mounted in the distributor cap.

8.6.11 Carburetors

8.6.11.1 One (two or four barrel) carburetor only with maximum spacer height of 1 ¼". Only 2 gaskets (1 per side) with a maximum thickness of 0.065 inches may be used on the adapter plate. Two carburetor return springs, mounted in two directions, required. No throttle cables allowed must be mechanical linkage. Toe strap on gas pedal required

Optional 141 Vortec Head engine, 4412 carburetor rule.

A. Carburetor Body - No polishing, grinding, or drilling permitted. Factory type air bleeds only. Screw in air bleeds are prohibited.

B. Choke - The choke may be removed.

C. Choke Horn - The choke horn may not be removed.

D. Boosters - The boosters and booster location may not be changed. The size or shape must not be altered. Boosters may not be tapered. Height must remain standard.

E. Venturi - Venturi area must not be altered. Casting ring must remain.

F. Base Plate - Base plate must not be altered.

G. Butterflies - Butterflies must not be thinned or tapered. Retaining screws may not be altered.

H. Throttle Shafts - Throttle shafts must not be thinned.

I. Metering Block - Only metering block 134-137 is permitted. Adjustable metering blocks are prohibited. Metering block must not be altered.

8.6.12 Cooling System

8.6.12.1 Stock cast iron or aftermarket aluminum water pump allowed. One radiator only and must be in front of engine. Metal shroud must cover top 180 degrees of fan. 2 quart overflow can mandatory. No devices that spray liquid onto radiator. **ABSOLUTELY NO ANTIFREEZE!**

8.6.13 Starter

8.6.13.1 Mini-Starter allowed. No reverse mounted starter. Must be able to start car

8.6.14 Engine Location

8.6.14.1 Engine must be centered between frame rails. Engine set-back to be measured as 33 ½" from top of steering box mounting bolt to rear of engine and 15 ½" from steering box mounting bolt to #1 cylinder spark plug. Crankshaft centerline can be no less than 7 ½" from bottom of front cross member. Non Chevy motor setbacks to be determined by the difference between the measurement from the center of top steering box mounting bolt on the frame to the back of the engine minus the measurement from the back of the engine to the center of #1 factory spark plug hole.

NO TRACTION CONTROL DEVICES ALLOWED!

8.7 Transmission and Drive Train

- A. 3 or 4 speed unaltered U.S. manufactured passenger car or light truck transmissions only
- B. Aftermarket racing transmissions, (Bert, Brinn, Falcon, etc.) or converterless transmissions are prohibited
- C. Steel blow proof bell housing only. Upper 270 degrees of bell housing must remain intact.
- D. Flywheel to be stock or aftermarket steel or aluminum unaltered except for balancing and resurfacing.
- E. No cut down stock flywheels allowed.
- F. **7 ¼" or 10 ½" diameter, steel or aluminum closed or open style, single or multi disc clutches allowed. No lightweight, spoke type flex plates allowed. OEM or OEM replacement flex plates only. V-drive clutch allowed.**
- G. Drive shaft to be magnetic steel, minimum 2 1/2" O.D., painted white with car # marked on it. Steel safety loop of minimum 1" x 1/8" thickness located within 1 foot of front U-joint.
- H. Stock rear end for frame used or Ford 9" floater recommended. Any combination okay as long as parts is steel. No aluminum parts except drive plates and spools. No offset. Drive flanges to be of equal distance on both sides of frame. No independent rear end (Corvette, Jaguar). Rear trailing arms must remain mounted in stock frame locations, NO EXCEPTIONS! One hole upper and lower for each trailing arm mount location, NO EXCEPTIONS! No aftermarket limited-slip or ratchet style rear ends (Ex: Gold Track, True Track, or Gleason, etc....).
- I. No gun-drilled axles.

8.8 Suspensions and Steering

- A. Steering wheel to be quick-release style.
Reference numbers to match dimensions are Moog part# D909 for center link, ES2034RL for inner tie rod ends, ES2033RL for outer tie rod ends, and K6187 for the idler arm.
- B. Stock steering box and linkage only. Steering quickener allowed. Steering shaft must have 2 u-joints or be collapsible style.
- C. Stock metric lower "A" arms only in stock locations, NO EXCEPTIONS! Stock unaltered metric spindles and rotors except for wheel bolt pattern and hole for outer tie-rod end. No aftermarket or dropped spindles. **One steel unaltered shock per wheel at any mounting position. No externally adjustable, coil over, air, or remote reservoir shocks. No Schroeder or gas refill ports. No coil over eliminators.** A Coleman hub may be used on the front only.
- D. All A-frame, trailing arms, steering components and suspension mountings must remain stock for frame in factory frame position. Outer tie-rod ends may be replaced by 0.625 or larger steel heim joint with a solid (no seam) steel adjuster sleeves and jam nut only. Rear trailing arms may be aftermarket, but must be stock length and use bushings. No heims, mono balls or adjustable. Length measured center-to-center, upper 11-1/8", lower 19-1/4". Must be same measurement side to side.
- E. ALL BALL JOINTS MUST BE STOCK FOR FRAME USED, NO EXCEPTIONS! If a metric frame is used must use stock dimension metric ball joints only, NO EXCEPTIONS!
Reference numbers to match dimensions are Moog part# K5208 for upper ball joint and K6145 for lower ball joint.
- F. Any OEM type sway bar permitted.
- G. Front upper control arm may be replaced with complete steel tubular drop in replacement, one-piece control arm of stock length (8"), plus or minus ½" of stock length using stock ball joints in stock location, NO EXCEPTIONS! No cutouts or modifications allowed. No alum components allowed, no adjustable cross shafts and slotting allowed, if slotted upper A arms are used you must weld a washer on the slot as not to be able to adjust control arm. No bearings, bushings or heim joints allowed.
- H. No aluminum components allowed except for steering quickener.
- I. Rubber Bushing Replacement – Stock or plastic and greasable bushings allowed. **No offset or bearing type (mono-ball) allowed.**

8.9 Brakes

Four-wheel foot operated hydraulic brakes in working order. Dual master cylinder allowed. Limiting or proportioning valves okay. GM Metric frames must use unaltered GM Metric calipers only. No aluminum, aftermarket caliper replacements.

8.10 Fuel and Tanks

- A. A. Pump gas only, E85 allowed, no alcohol or additives.
- B. B. Approved fuel cell to be enclosed in 22-gauge sheet metal with 2 straps in both directions. Straps to be 1" x 1/8" steel. Ground clearance of 10". Fuel cell protection bar on the bottom (as low as the fuel cell). Minimum of 1 1/2" x .065 wall square tubing fuel cell hoop with two or more braces to frame (One on each side of fuel cell). Fuel cell to be filled from top and have approved cap, no bottom draw fuel cell, outlet must be on top. No fuel lines in driver area.
- C. C. Stock style fuel pump in stock location. No electric or belt driven pumps.
- D. D. No bottom draw fuel cells. Outlet must be on top.
- E. E. The fuel cell breather or vent must have a one-way check valve to prevent spillage in case of a wreck or rollover and below bottom of tank.

8.11 Bumpers and Protection Bars

- A. Stock bumpers may be reinforced; ends must be strapped to fenders. Must have both bumpers to race. If rear bumper falls off during competition, protection bars must protect the fuel cell. Radiator protection bar may be no higher than the front of the hood or exceed 8" above the frame rail and no wider than the frame rails and may not extend beyond the front of the bumper. Trunk protection bar no higher than the rear of the trunk lid. Protection bars to be 1 3/4" x .095" wall round tubing or smaller. Side protection bars to be no larger than 1" x .095" wall square tubing with the ends capped at 45-degree angle. **On doors only, may run lexan rub rails no more than 6" tall and protruding out 1" from door.**
- B. All cars must have a loop, hook or other provision in the centerline of the front and back, behind the bumper line for the wrecker to hook on to.

8.12 Wheels and Tires

- A. A. Racing steel wheels of 8" maximum width, minimum weight of 19lbs. NO wide 5 wheels. Bead locks are allowed on right side of car only!
- B. B. 1" hex steel lug nut mandatory. Studs must pass through lug nut completely.
- C. C. Hoosier "E-MOD RC-4" tires only. No grooving, siping, or softening allowed. Tire grinding allowed with a 16 grit or finer fiber disc. (NO steel discs).
- D. D. Foam type or soft plastic mud plug allowed on right side tires only

8.13 Electrical

12-volt electrical system only, one 12-volt battery only, which must be located behind driver area between frame rails. Battery must be no lower than frame rail and securely fastened. An On/Off and start switch to be clearly marked in driver area and easily reached by driver. A second battery kill switch must be located behind driver easily reached by safety crew and KILL ALL POWER TO CAR!

8.14 Weights

Minimum car weight 3000 lb. Weights to be bolted on securely, painted white with car number and division on each weight. No weights in driver's compartment allowed. **Loss of any weight during a race will result in a money penalty and disqualification!**

8.15 Mirrors

Absolutely no mirrors allowed

8.16 IMCA Stock Car crossover rule

IMCA Stock Cars must run their rules.

8.17 Transponder Location

See general rules for ALL transponder information