



WISCONSIN'S PREMIER 1/3 MILE CLAY RACE TRACK

460 E. County Rd Y
OSHKOSH, WI 54901



2012

OSHKOSH SPEEDZONE RACEWAY RULE BOOK

Official interpretation of these rules is made by Track Officials and is final. Amendments to these rules may be made at any time at the discretion of the Track Officials. Non-compliance may result in not being able to compete, loss of points, loss of monies, fines, and/or suspensions.

RULES DISCLAIMER

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules. **NO EXPRESS OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM PUBLICATIONS OF OR COMPLIANCE WITH THESE RULES AND/OR REGULATIONS.** They are intended as a guide for conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, officials, or others. The race director shall be empowered to permit minor deviation from any of the specifications herein or impose any further restrictions that in his/her opinion does not alter the minimum acceptable requirements. **NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM SUCH ALTERATION OF SPECIFICATIONS.** Any interpretation or deviation of these rules is left to the discretion of the officials. Their decision is final.

No part of this publication may be reproduced, stored, or transmitted in any form, without prior written permission of Impact Motorsports LLC / Oshkosh Speedzone Raceway.

**IMPACT MOTORSPORTS LLC
D.B.A. OSHKOSH SPEEDZONE RACEWAY
PO BOX 3226
OSHKOSH, WI 54903-3226**

DAY OF EVENT, TRACK NUMBER - 1-920-279-9096 / 920-252-1463
Website - www.oshkoshspeedzone.net
Email – oshkoshspeedzone@yahoo.com

REPRESENTATIVES:

Competition Director –

Modified Rep. – Dennis Baldry – 920-203-0296 (Cell)

Grand National Rep. – Jerry Winkler – 920-203-6455 (Cell)

Street Stock Rep. – Louis Reichenberger – 920-426-0523 (Home)

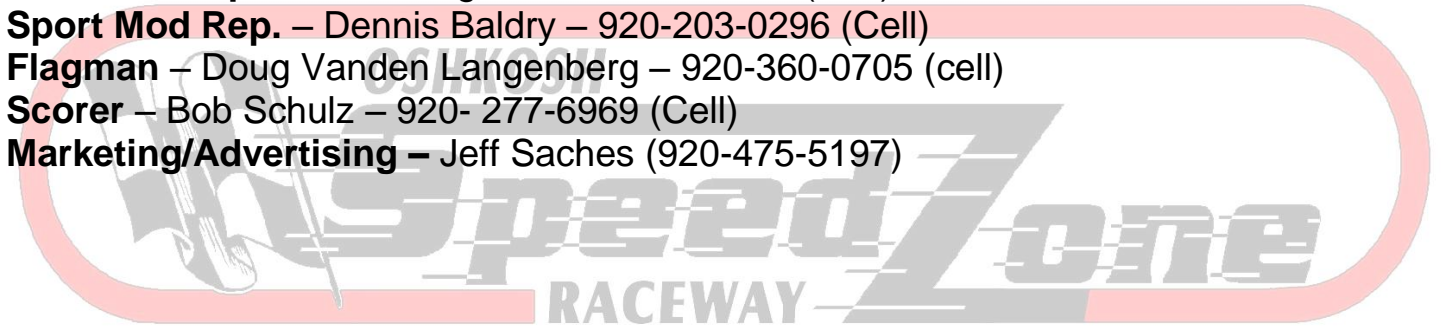
Dirt Devil Rep. – Mike Morgan – 920-427-8373 (Cell)

Sport Mod Rep. – Dennis Baldry – 920-203-0296 (Cell)

Flagman – Doug Vanden Langenberg – 920-360-0705 (cell)

Scorer – Bob Schulz – 920- 277-6969 (Cell)

Marketing/Advertising – Jeff Saches (920-475-5197)



IMPACT MOTORSPORTS LLC – OWNERS

JEFF LEMIESZ – 920-279-9096 (Cell)

LARRY STRATTON – 920-252-1463 (Cell)

REVISIONS/CLARIFICATIONS UNDERLINED

**DIAGRAMS IN THIS BOOKLET ARE FOR REFERENCE ONLY WITH THE
EXCEPTION OF THE MODIFIEDS**

SECTION - 1

2011 OSHKOSH SPEEDZONE RACEWAY OPERATION RULES

1.1 SCHEDULE OF EVENTS

4:00 – Pit gates open.

6:00 – Race car registration deadline for lineups or you will be considered late.

6:00 – Mandatory Drivers Meeting. If not present you will be put in the back of your heat. Roll call may be done.

6:15 – Track Packing if needed – If you do not pack when asked you will start all events in the back row.

6:25 – Opening Ceremonies; National Anthem with parade lap of previous weeks feature winners.

6:30 – Racing starts.

1.2 ANNUAL RELEASE FORM

An annual release form must be filled out prior to admittance to the pit area. Annual release card must be shown prior to admittance to the pit area. If you do not have an annual release form filled out, you will be asked to fill one out and sign the waiver.

1.3 PIT AREA

Anyone under 12 years of age must be accompanied by a parent or guardian to enter the pit area. Anyone under 18 years of age must have an insurance minor release form filled out, signed and on file with Impact Motorsports LLC before admission.

Wrist band must be worn on left wrist at all times. Drivers must purchase and wear a wrist band for each division that they are competing in. If you are caught in pit area without wrist band you will be fined \$50.00 plus suspended from pit area until fine is paid. Racecar driver is the sole spokesman for the car in any and all matters pertaining to an Event, and is responsible for the actions of the pit crew, and owner.

No alcoholic beverages may be opened in the pit area until after the entire program is completed – any violation will result in being ejected from the track, loss of points and/or earnings may also be forfeited.

Proper footwear required in the pit area.

1.3.1 ATV, GOLF CARTS & MOTORCYCLES

ATV's & Golf Carts will be allowed; ATV's and Golf Carts must register with the Speedzone and show proof of insurance. Registration forms will be available online and through Impact Motorsports LLC.

Motorcycles and bicycles are prohibited from the pit area, unless prior arraignments have been made or allowed by touring series.

1.3.2 PIT STALLS

Front row pit stalls can be reserved for \$100 for the season (including specials). Prior rented stalls will be available to previous year's renters until the end of January of the upcoming year then on a first come first paid basis. Any other spot may be reserved (including specials) for \$50. You must supply your own pit stall sign designating your reserved spot.

1.3.3 WASTE PRODUCTS

No dumping petroleum products, anti-freeze, tires, etc. on track premises – any violation may result in fines, loss of points, and/or loss of monies. Any competitor that spills antifreeze, or any non-approved chemical, on the track or in the pit area, will be held responsible for all investigation and clean up costs.

1.4 DRIVERS

Drivers must be at least 14 years of age to drive in the 4 cylinder division.

Drivers must be at least 16 years of age to compete in all other divisions. Drivers younger than 16 wishing to compete in other divisions must be approved by track Officials.

Any driver under 18 years of age must have an insurance minor release form filled out, signed and on file with Impact Motorsports LLC before admission. Drivers must purchase and wear a wrist band for each division in which they are competing.

Drivers are considered independent contractors and assume responsibility for all charges, premiums and taxes, if any, payable on any funds received as a result of participation in any events. Impact Motorsports LLC and sponsors reserve the right to use photos of any driver or their car for promotional purposes.

Drivers must wear full face helmet and seatbelts whenever racecar is on the track. The only exception is during the National Anthem.

1.5 TRACK PACKING

All cars in all divisions are required to assist in packing the track when needed. When a division is called to pack the track, all cars in the pit area must report to the track. Any cars that do not assist in packing the track will start all races in the last row of their race for the entire Event. Alternate vehicles may be used for track packing, but must be registered with the Speedzone. One track packing vehicle per car. Packing vehicle must utilize the same tires as the division and display the car number. Track packing vehicles kept at the track must park in designated area and leave key. **ALL vehicles must be in running order!**

1.6 MUFFLERS

All cars must be equipped with a muffler. Cars cannot exceed a noise level decibel reading of 95 measured at 100 feet from the outside of track. Any car with noise levels that contribute to a violation of the community noise limit: loss of points and money for Event; and/or accumulated points for current year.

1.7 CAR NUMBERS

All numbers will be registered with Impact Motorsports. All numbers must have a letter. The letter must be at least A **6" OR TALLER**. All numbers on the doors must be a minimum of 20" tall and 3" wide. A **6" OR TALLER NUMBER REQUIRED ON FRONT AND BACK OF CAR**. Numbers must be in a contrasting color. No metallic numbers. Numbers must be kept clean and to be between 00 and 99. Failure to meet these requirements you risk the chance of not being scored or allowed to compete. A number is required on the roof, readable from the right side, and must be as large as possible. IT IS THE RESPONSIBILITY OF THE DRIVER TO ENSURE THAT THE SCORERS CAN READ THE CAR NUMBER. IF YOUR CAR NUMBER IS NOT READABLE FROM THE SCORING TOWER, THE CAR WILL NOT BE SCORED UNTIL THE NUMBER IS CORRECTED.

Track Officials reserve the right to assign or restrict the display of graphics and advertising on race cars. Offensive graphics or slogans are not permitted. All Competitors agree to accept Track's decision in this matter.

Where required, participating sponsors emblems, decals, or Track technical stickers, will be placed in the position designated by Track Officials. Cars that do not display all sponsors emblems, or decals, will receive less prize money.

1.8 BALLAST WEIGHT

Ballast weights must be painted white with car number clearly marked on it.

1.8.1 IF WEIGHTS FALL OFF CAR

a) FIRST OFFENSE:

Car will be black flagged for that event, plus a \$1.00 dollar per pound lost fine.

b) SECOND OFFENSE:

Car will be disqualified for that event plus a \$2.00 dollar per pound lost fine.

1.8.2 DRIVE SHAFT

Drive shaft must be painted white and car number on it.

1.9 DIAGRAMS

Diagrams published in this rule book are intended for reference guidelines only unless otherwise noted. The only exception to this rule will be in accordance of the Modified division. The Modified division will continue to follow the rules as written in this rule book.

1.10 DRIVERS MEETING

All drivers, or a representative from each car, must attend the drivers meeting. Attendance may be taken. If the driver, or car representative, is not present, the car will start all events in the rear that the car qualified for. Each car will be exempt from this rule its first night at the track to accommodate newcomers.

1.11 TRANSPONDERS

Transponders are required on all race cars. It is the driver's responsibility to make sure the transponder is in the correct placement and secured. If a transponder falls off during competition, the penalties are as follows:

a) First offense

Heat race - black flag and put in „B" feature.

Feature - black flag and scored in last place with that place points and money

b) Second offense

Heat race and/or feature - black flag and loss of points and money for night.

c) Third offense

Suspension and fine, discretion of track promoter and Track Officials.

If transponder stops working you will be scored until verification of missing or broken transponder is clarified. Practices will be used to test and record transponder locations. IT IS THE RESPONSIBILITY OF THE DRIVER TO ENSURE THAT THE TRANSPONDER IS FUNCTIONING PROPERLY. IF YOUR TRANSPONDER IS NOT FUNCTIONING PROPERLY, THE CAR WILL NOT BE SCORED UNTIL THE PROBLEM IS CORRECTED.

1.11.1 TRANSPONDER LOCATIONS

1.11.1.1 Modifieds, Grand Nationals, Street Stocks and Sport Modifieds must be placed twenty four inches (24"), or more, behind the center line of the rear axle to the forward edge of the transponder, on the right side frame rail.

1.11.1.2 Dirt Devils are to be located in the spare tire well area recommended 12", or more, behind the center line of the rear axle to the forward edge of the transponder.

Note: No metal of any kind can be between transponder and track. Transponder pouch should be placed horizontally with no obstructions between the pouch and the ground. If transponder pouch is mounted to round tube frame components with nylon ties, secure the transponder pouch so that it cannot spin from its horizontal position. This may be done by duct taping the nylon ties to the frame rail.

All transponder locations must have a clear view of track for signal to transmit.

1.12 PIT CLOSING TIME

The pit area will be closed at 12:00 am, (Midnight). Failure to clear the pit area within this period will result in the loss of prize money, and/or points. The amount of prize money, and/or points that will be lost is the decision of the Competition Director, and/or track management, which is final. All drivers are responsible for keeping their pit stalls clean. Track management reserves the option to assess pit stall cleanup costs for failure to keep pit stalls clean and use proper waste disposal containers.

SECTION – 2 SAFETY

2.1 TRACK SAFETY

Racing is an inherently dangerous sport. Each Competitor assumes the risk of injury or death when he/she participates in an Event. Competitors are solely, and directly, responsible for the safety of their race cars and racing equipment. OSHKOSH SPEEDZONE RACEWAY IS NOT RESPONSIBLE FOR THE ADEQUACY OF A COMPETITORS RACE CAR OR RACING EQUIPMENT. NO EXPRESS, OR IMPLIED, WARRANTY SHALL RESULT FROM THE PUBLICATION OF, OR COMPLIANCE, WITH THESE RULES. These rules govern the conduct of an Event, and, by participating, Competitors are deemed to be in compliance with these rules.

2.2 SAFETY EQUIPMENT

A full face, helmet and face shield, Snell-rated SA2000, SFI 31.1/2005, SA2005 or SA2010 required or manufactured build date for no longer than a 10 year time period and MUST be in good condition. Helmet and face shield must be worn at all times car is on the track and must accompany vehicle at time of inspection. Roll bar padding required in driver compartment (*Fire retardant recommended*).

2.2.1 REQUIRED

SFI-approved full fire suit, fire retardant gloves, shoes, driver-side window net, minimum 16 inch by 20 inch ribbon or mesh style, and must be mounted to roll cage so latch is at top front of window.

Seat belt and shoulder harness recommended being date stamped, 2010, 2011 or 2012 and being in good condition. (Y-type shoulder harness prohibited.) Belts must not be frayed. Seat belt and shoulder harness must be installed according to manufacturer's recommendations. Lap and shoulder belts must be approved snap-type release belts at least three (3) inches wide and securely fastened to the frame and or roll cage. A crotch or anti-submarine belt is mandatory, and must be a minimum two (2) inches wide.

2.2.2 RECOMMENDED

Neck brace or head and neck restraint. Right and left seat head supports required if using head restraint system with no neck collar. Fire retardant head sock and underwear; collapsible steering shaft recommended. . IF NO NECK BRACE IS USED A FIRE RETARDANT HEAD SOCK MUST BE USED!

2.3 DISCONNECT SWITCH

A kill switch required within easy reach of driver and must be clearly marked „OFF“ and „ON“.

A second battery disconnect switch, must be mounted behind or next to driver on deck cover and within reach of the safety crew. The disconnect switch must shut off all power to the car and be brightly colored. The battery must be securely installed. The positive terminal must be covered.

2.4 HEAD CLEARANCE

All divisions must adhere to the 2 inch head clearance rule!

Measurements will be taken from top of helmet to top of top roll bar with driver strapped in race ready!

SECTION - 3 PENALTIES

3.1 PROCEDURE

If a Raceway Technical Official observes, or is made aware of, a violation of the Raceway Rules, by a Competitor, Raceway Officials will investigate claim and can impose an appropriate penalty if found to be true.

3.2 EMERGENCY ACTION

If an act by a Competitor is determined by Raceway Officials, Raceway Technical Officials, or persons serving under Raceway direction, to threaten the orderly conduct of an Event, the Raceway Officials can take emergency action against the Competitor. Such action may include: arrest; ejection from the racing premises; suspension from competition; or any other action to remove the threat created by the Competitor. Examples of conduct warranting emergency action include, but are not limited to: consumption of alcoholic beverages in pit area before or during an Event; use of illegal drugs before or during an Event; harassment, verbal abuse, or assault of any Raceway Officials, Raceway Technical Official, or Competitor; fighting; reckless driving; and failure to obey a black flag or directions of a Raceway Official. The emergency action will remain in effect for the period of time determined by the Raceway Officials, except for an ejection which is final and non-appealable.

3.3 PAYMENT of FINES

Fines must be paid to Oshkosh Speedzone Raceway and will be deposited in the current discretionary fund. Failure to pay fines may result in suspension from competition. All unpaid fines may be collected, by Oshkosh Speedzone, by deducting the amount from the purse or point fund. If the Competitor is not a driver, the fine may be deducted from the purse or point fund of the driver with whom the competitor is associated at the time of the violation. Any unpaid fines remaining at the end of the racing season will be carried over to the next racing season and be deducted from the purse or point fund until all monies are collected.

3.4 SCOPE of PENALTIES

Penalties for violation of Raceway Rules are determined by the severity of the violation. Penalties include, but are not limited to: fines; loss of points; loss of purse; disqualification; suspension of driving privileges. Oshkosh Speedzone Raceway will use the following guidelines for determining penalties. **A greater or lesser penalty may be imposed depending on the circumstances.**

3.5 GENERAL

- a) Any Competitor who performs an act or participates in an act deemed by Track Officials as detrimental to auto racing or the Track: a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- b) Any Competitor who signs the release sheet for anyone else: ejection.
- c) Any parent or legal guardian that falsifies an insurance waiver for a minor Competitor: ejection of the minor Competitor; and/or suspension of the minor Competitor; and/or loss of points and money for Event; and/or loss of accumulated points for current year; and a fine determined by Track Officials.
- d) Any Competitor who harasses, verbally abuses, or assaults any Track Official, Technical Official, or persons serving under Track direction: ejection; a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- e) Any Competitor who participates in fights on the racing premises: ejection; a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- f) Any Competitor who, while participating in a Speedzone Event, consumes any alcoholic beverages and/or illegal drugs, or is under the influence of alcohol and/or illegal drugs: a fine determined by Track Officials; ejection & disqualification; and suspension for a period of time determined by Track Officials.
- g) Any Competitor who stops on the track to argue with a Track Official: a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- h) Any Competitor who fails to correct an unreadable car number: stop scoring of the car until the number is corrected; and/or a fine determined by Track Officials.
- i) Any driver not wearing a full driver's suit and/or gloves and/or other mandated driver safety equipment during an event: "disciplinary" black flag; and/or a fine of \$25.00.
- j) Any Competitor who leaves tires in the pit area or anywhere on the track property: a fine of \$25.00/tire.

3.6 ROUGH DRIVING

- a) Disciplinary black flag, fines, suspensions and loss of points based on severity of incident is at the discretion of track officials.
- b) Intentional use of vehicle in a malicious manner - minimum \$1000 fine and/or suspension is at the discretion of track officials.

3.7 FUEL

Sample may be taken from any car at anytime.

- a) First violation - \$250 fine.
- b) Second violation - 3 event suspension plus the cost of inspection fees. Only completed Events are counted towards suspension.

SECTION - 4

TRACK PROCEDURES

4.1 RACE DECISIONS

All decisions by Oshkosh Speedzone Raceway Officials involving track procedures are final, and non-appealable.

4.2 RAIN OUTS

Raceway Officials will consult with the track owner to determine if the Event will be postponed.

In the event of a rain out prior to the opening of the pit gate, information can be obtained by calling the race track. The track phone number for "Oshkosh Speedzone Raceway" is **(920) 252-1463 / (920) 279-9096**.

In the event of a rain out after the pit gates are open, all qualifying races and B Main races must be completed in order to receive points and purse. Completed races will receive full points and purse. Uncompleted A Main races will receive total points and purse for the race, divided equally among the race cars. Rain out passes, if issued, or wrist bands from the event, are good only for the next two (2) completed events at the track where the rainout occurred.

4.3 WEIGH IN

All drivers are encouraged to weigh their race car, prior to racing. Any race car under the allowable weight is subject to disciplinary action. Penalties will be imposed in accordance with **SECTION 3 - PENALTIES**. Random weight checks may be conducted at any time. Weights will be determined by the track scale, which is considered official. The top five race finishers in all divisions, or as designated, must report to the Claim/Inspection Area immediately after the race. Winners may go into victory lane without penalty then to scaling.

4.4 LINEUPS

It is the driver's responsibility to make sure your car is **REGISTERED AND SIGNED IN** and car number is on the lineup sheet. Failure to do so will result in being placed at the rear of the heat lineups.

All cars must be lined up in the staging area in the proper position, when the front row cars begin to move out of the staging area. Cars entering the staging area or race field after the front row cars begin to move must start the race at the rear in the order they join the field. Exceptions will be made for drivers competing in more than one division per night, or a Competitor who notifies a pit steward of extenuating circumstances.

4.5 CAR AND DRIVER CHANGES

A driver may drive a different car that has been qualified but will start the event from the rear. If a driver qualifies for the feature but needs to use an unqualified car to compete, the driver must race through the "B" main to qualify for the feature. The driver will start in the last place position of the "B" main.

ALL DRIVER AND CAR CHANGES MUST NOTIFY ONLY THE COMPETITION DIRECTOR, COMPETITION DIRECTOR WILL NOTIFY SCORING TOWER OF CHANGES!

4.5.1 DRIVER SUBSTITUTION

A driver may have a substitute driver once a year. Original driver must notify registration crew one week in advance. Registration crew will make note of the change.

4.6 CONDUCT

The driver is the sole spokesperson for the car in any and all matters pertaining to an Event, and is responsible for the actions of the pit crew, and owner. Determination of crew affiliation will not be limited to the pit sign in sheet. It will be a determination made by the track staff. Harassment or abuse of Raceway Officials will not be tolerated and is subject to disciplinary action.

4.7 INJURIES

All injuries must be reported to a Raceway Official prior to leaving the race premises. Track insurance will not cover any unreported injuries. Any driver involved in a roll over, or severe accident, must be checked and cleared by the safety staff prior to returning to the track.

4.8 CAMERAS

In car cameras will be allowed for personal use only and are thus deemed unofficial. Unofficial cameras will not be used to change or determine any calls made before, during, or after a race event.

4.9 SAFETY VEHICLES


Any driver that hits any track vehicle will pay for damages!

SECTION - 5 POINTS/PAYOUT PROCEDURES

5.1 POINTS

Weekly „A“ feature win is 40 points with each subsequent position worth one point less, with 24th position in feature worth 17 points. If track runs a „B“ feature(s), first non-transferring driver receives 16 points and each subsequent position, back to sixth position, receives one less point per position. Drivers that do not qualify for „A“ or „B“ feature will not receive points or money. Heat points are awarded. 1st - 3 points; 2nd - 2 points; 3rd - 1 point.

1. 40	Transfer	Heat 1 - 3
2. 39	Transfer	Heat 2 - 2
3. 38	Transfer	Heat 3 - 1
4. 37	Transfer	
5. 36	16	
6. 35	15	
7. 34	14	
8. 33	13	
9. 32	12	
10. 31	11	
11. 30	11	
12. 29	11	
13. 28	11	
14. 27	11	
15. 26	11	
16. 25		
17. 24		
18. 23		
19. 22		
20. 21		

- 
- a) Drivers qualifying for the „A“ main and not be able to compete when there is a „B“ main. He/she must notify competition director and will receive last place points and money. If he/she does not notify the competition director, he/she may not get any points or money. It is promoter's discretion if vacated position is filled by next qualified driver.
- b) Drivers qualifying for the „B“ main must take the initial green flag and complete one lap. They will be scored in last place position and receive points and money accordingly. If there are not enough cars for a „B“ main, all heat cars transfer to „A“ main. Driver must take initial green flag and complete one lap in feature to be awarded points and money.
- c) Points remain with the driver, not with the car.
- d) Divisions that have less than ten entries, one point fewer than normal is awarded for each car less than ten. For example, if nine cars entered, feature win would then be worth 39 points; for eight entries, win would be worth 38 points, etc.
- e) Points will determine track championships. In case of a tie, feature wins will be counted, then seconds, thirds, etc.

5.2 PAYOUT

All positions in features get money and/or a pass. „B“ mains receive money if the initial green flag and one lap is taken. Heat races do not pay – they are for points and qualifying for „B“ and „A“ features. Earnings are paid out at the following event from 4:00 to 6:00 in pole shed. Any unclaimed earnings will be available the following week during the same time period. Any driver wishing his check mailed must provide a stamped self addressed envelope (SSAE). Any money not claimed after **30** days will be returned to Impact Motorsports LLC.

Driver must compete in seventy percent (70%) of scheduled events and attend awards banquet to be eligible for point fund.

5.2 LINE-UP PROCEDURES

Heat races will be lined up according to the starting position drawn by the driver, for the first Event only. After the first Event, qualifying races will be lined up according to average Speedzone driver points, with the lowest point average drivers to the front and the highest point average drivers to the rear. Drivers without Track points will be placed in the last row. If a driver is absent for two consecutive events (excluding rainouts), they will be treated as a new driver the first week they return. The deadline for race car registration and lineup is 6:00 PM, or you will be considered late and placed in the last row.

Point averages are = (total points) / (number of events competed in).

For the first night of racing all drivers draw two numbers. The first number is for heat starting position. The second number is the redraw for „A” feature starting positions. One heat - 5 redraw; two heats - 10 redraw; three, four, five, or six heats - 12 redraw.

„B” features are lined up from heats with highest finishers at the front.

‘A’ features: Heat qualifiers are lined up by inverted point average. When one heat is run the top 5 qualifiers from the heat are inverted. When two heats are run the top 10 qualifiers will be inverted. When three, four, five, or six heats are run the top 12 qualifiers will be inverted. The remaining heat qualifiers are lined up behind inverted field as they finished. A driver that qualifies for the invert but has no point average shall be lined up in the last invert position. Should there be more than one driver with no point average; they will be lined up at discretion of officials. Impact Motorsports LLC also reserves the right to relocate a driver to the rear of the field if deemed necessary for the safety and well-being of all involved. Remainder of „A” feature is lined straight up from heats and/or „B” features. If a driver is absent for two consecutive completed events they are treated as a new car (zero point average) for the first week he/she returns. Total points are not lost.

5.4 LAPS

Heats - 1 lap per car with a maximum of 10 laps, or 1 minute per lap.

„B” features will normally be 1 lap per car with a maximum of 15 laps, or 1 minute per lap.

„A” features will normally be 20 laps.

Track officials have the option to alter race length based on car count, weather, time, etc. Race will be scored complete after laps are completed or time limit is up.

5.5 RACING

Cars must enter the track from turn three and leave the track using the turn two exit.

On original starts the front two cars are double file and pace the start together with moderate speed. These front row cars are given 2 chances at a clean start before both cars being sent to the rear of the starting field. No jackrabbit starts allowed – flagman’s discretion. Any car(s) causing a yellow flag before the initial start of the race will restart the race in the last row.

All restarts will be double file, except for the leader who will be alone in the first row. The car leading the race controls the restart. A restart cone will be placed at the exit of Turn 4. After the “one lap to go” signal is given, and/or the drivers are notified over the Raceciever, all cars must stay nose to tail. Lagging back is prohibited. On all restarts, all cars must maintain their position and may not accelerate until the lead car passes the restart cone. Any cars lagging back prior to the restart, hitting the cone, or passing to the left of the restart cone, will be penalized two positions on the next caution. Any cars passing prior to the lead car passing the restart cone will be penalized two positions per position advanced on the next caution. If no caution occurs, the driver will be penalized at the end of the race. Any car(s) causing a yellow flag before the restart of the race will restart the race in the last row. Single file restarts will be at the discretion of the flagman.

Any single driver spinning out unassisted and causing one (1) yellow flag in a heat race, or two (2) yellow flags in the „B” feature or „A” feature race, will receive a disciplinary black flag. This does not apply to a driver that spins out to avoid a wreck. A driver that spins out to avoid a wreck and does not make contact with the wall or any other vehicle may receive their position back. The determination of whether a driver spins to avoid a wreck is a judgment call by the flagman, which is final.

Driver must use pit lane if he/she wishes to re-enter the race. Any car on pit lane will rejoin the field from the rear. Driver must pull over to the right as much as possible while on pit lane. If a car pulls off the track in turn three the car is considered in the pit and out of the race.

If a driver enters pit lane for repairs, repaired part must be checked by pit lane person staged at the end of the pit lane. If driver fails to stop, he/she will be black flagged and not allowed to re-enter event. Lapped cars will start race at back of field.

No one but the driver is allowed on the track at any time during an event. Driver will be disqualified from the event if pit crews enter the track unless specified by track officials. Driver will lose all points and pay for that event.

No unapproved communication, recording, or traction control devices will be allowed in any car.

A SW1600 model Raceciever, tuned to channel 0000 (454.000) is mandatory for all divisions.

5.6 STARTS AND RESTARTS

On ALL restarts, drivers should line up single file front to back on caution laps to give scoring time to put all cars in correct position.

Once positions are correct, drivers will be notified via Receiver to form two wide with leader in front by him/herself. On all new starts, two wide front to back. In the event the two front cars cannot make pace, at the flag mans discretion, the two cars may be sent to the back of the field and the row will just move ahead. On all starts and restarts, at the flag mans discretion, if the start cannot be accomplished, he may make a single file start. We will use the acceleration cones in the apex of corner 3 & 4 with the official starting cone at the end of corner 4.

5.7 FLAGS

5.7.1 GREEN FLAG

The green flag, and/or green light, indicates the start of the race or the restart of the race after a caution period. The green flag and green light will be displayed as the lead car(s) pass by the flagman. All race starts will be double file, nose to tail. ("Nose to tail" means staying in the tire tracks of the car ahead of you.)

5.7.2 WHITE FLAG

The white flag indicates that there is one lap remaining. If a yellow flag occurs after the white flag is displayed and the leader has crossed the start/finish line, the race is considered complete. All cars that have passed the start/finish line will be scored as they crossed the start/finish line. All remaining cars, **except** those involved in the incident, will be scored according to their last completed lap. The cars involved in the incident will be scored at the end of their last completed lap.

5.7.3 CHECKERED FLAG

The checkered flag indicates that the race is complete. Once the checkered flag is displayed to the race leader, the race is considered complete. All cars receiving this flag must slow to a reasonable speed and, with the exception of the winner, return to the pit area. Failure to slow to a "reasonable speed" may result in disqualification. The determination of reasonable speed is a judgment call by Track Officials, which is final. If the yellow flag occurs after the checkered flag is displayed to the race leader, all cars that have passed the start/finish line will be scored as they crossed the start/finish line. All remaining cars, **except** those involved in the incident, will be scored according to their last completed lap. The cars involved in the incident will be scored at the end of their last completed lap. Race winners will report to the "winner's circle" for post race ceremonies. Finishing positions will be determined by the number of laps completed, whether the car is running, or not. Effective Date: January 1, 2010

5.7.4 YELLOW FLAG

The yellow flag, and/or yellow light, indicates caution on the track. All cars receiving this flag, and/or light, must slow, hold their position, and form a single file line behind the lead car. **Absolutely no racing back to the yellow flag.** The penalty for racing back to the yellow flag, as determined by the flagman, is restarting the race at the rear. If a pace vehicle is used, the leader must line up behind it. Cars will be lined up as they were scored on the last completed lap. Lapped cars will be placed in the back of the field. Any cars entering pit lane during the caution period must restart the race at the rear, in the order they return to the track. Re-entry under the yellow flag is not permitted until the lineup is complete. If the yellow flag occurs before the completion of one lap, the cars will be lined up in their original starting order **except** those cars involved in the incident, which must restart the race at the rear. Any driver, as determined by the flagman, intentionally causing a yellow flag (such as spinning out another car, stopping on the track, or capable of resuming the race after a spin but does not) will receive either a one lap penalty or "disciplinary" black flag. No car may pass the pace vehicle unless directed by a Raceway Official. Any car illegally passing the pace vehicle is subject to the black flag. Prior to restarting the race, the flagman will signal one lap to go.

5.7.5 BLUE FLAG WITH DIAGONAL YELLOW STRIPE (FASTER TRAFFIC APPROACHING)

The blue flag with the diagonal yellow stripe indicates that faster traffic is approaching. Cars receiving this flag must prepare to yield to faster traffic. Cars should hold their line on the track and allow the faster traffic to pass. If the slower cars are running side by side, they must form a single line at the bottom of the race track. Failure to obey a "faster traffic approaching" flag is subject to a "disciplinary" black flag.

5.7.6 RED FLAG

The red flag, and/or red light, indicates that the race must stop immediately, regardless of the position of the cars on the track. During a red flag, pit crew members are not allowed on the track. All car repairs must be done on pit lane. If the red flag occurs before the completion of one lap, the cars will be lined up in their original starting order **except** those cars involved in the incident, which must restart the race at the rear. If the red flag occurs after one complete lap, the cars will be lined up in single or double file according to the last completed lap. Prior to restarting the race, the flagman will signal one lap to go.

5.7.7 BLACK FLAG

Pull off the track in turn two onto pit lane and Pit Steward will advise you the reason for the flag. If black flag is for a mechanical problem, car may return to track after making necessary repairs. If black flag is for disciplinary reasons, the car and driver are disqualified from race.

SECTION - 6 INSPECTIONS

6.1 INSPECTIONS / ENGINE EXCHANGE / PURCHASE / PROTEST / SEALING

Track officials have the right to inspect any engine or car at any time.

The Competitor will be given a "reasonable amount of time" to remove components from the car for post race inspection or claim. The Competitor will be notified as to what is considered a "reasonable amount of time" at the beginning of the inspection or claim. If the removal of the components is not completed in this time period, the car will be disqualified.

The cars crew and or driver will perform the actual teardown or engine removal with their own tools and equipment. Only two (2) of the cars crew and the driver will be allowed in the Inspection/Claim area.

The individual(s) representing the car during post race inspection or claim may not consume any alcoholic beverages, or be under the influence of alcohol, until the post race inspection of the car or claim is complete.

The top five race finishers in all divisions, or as designated, must report to the Inspection/Claim Area immediately after the race.

6.2 ALL CLAIMS, EXCHANGES AND PROTESTS WILL BE SUBJECT TO A \$50.00 ADMINISTRATION FEE.

6.2.1 MODIFIEDS

ENGINE CLAIMING RULES:

6.2.1.1 \$1,050 cash, or \$100 and exchange, claim on engine, flywheel and balancing plates (\$25 goes to wrecker and \$25 to official for each engine). Claimed driver has option of accepting cash, or exchanging engines with claiming driver.

6.2.1.2 Claim does not include - 1. Clutch, 2. Pressure plate, 3. Bellhousing, 4. Headers, 5. Carburetor, 6. Starter, 7. Motor mounts, 8. Oil/temp. Sending units, 9. Carburetor spacer, 10. Fan and pulleys, 11. Clutch ball, 12. Clutch arm, 13. Throw out bearing, 14. Dip stick, 15. Water pump, 16. Fuel pump, rod and plate, 17. Distributor, 18. Plug wires, 19. Water outlet and restrictor, 20. Breathers.

6.2.2 STREET STOCKS

Track officials have the right to purchase an engine for \$750.

Protest only, \$200 top, \$300 bottom, \$500 for complete engine.

6.2.3 GRAND NATIONALS

Protest only, \$200 top, \$300 bottom, \$500 for complete engine.

6.2.4 DIRT DEVILS

\$600 claim swap for entire car except for the seat, seat belts, window net and fuel cell (if equipped). Track officials reserve the right to buy any car for **\$1000** less the seat, seat belts, window net and fuel cell (if equipped).

6.2.5 SPORT MOD

ENGINE CLAIMING RULES

6.2.5.1 \$550 cash claim on engine, \$25 goes to wrecker for pulling engine and \$25 goes to official.

6.2.5.2 Claim does not include - 1. flywheel, 2. clutch, 3. pressure plate, 4. bellhousing, 5. breathers, 6. carburetor, 7. starter, 8. motor mounts, 9. oil/temp. sending units, 10. fan and pulleys, 11. clutch ball, 12. clutch arm, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor, 18. plug wires, 19. water outlet and restrictor, 20. headers.

NOTE: During engine claim, block and head numbers should be verified, heads, intake, connecting rods and crankshaft should be visually inspected and one spark plug must be removed to check flat top or dish pistons prior to transfer of engine to claiming driver. If, at this time, engine is found illegal, claimed driver is disqualified and suspended from all events until such time as a \$250 fine is paid, for first offense. Claimer then has option to accept or decline engine - if declined, driver is not charged with claim. Second offense penalties will be \$500 fine and/or suspension.

6.3 WHO CAN BE CLAIMED/PROTESTED

Top four finishers of the „A“ Feature whether running or not are subject to engine exchange claim / protested. The top four drivers must proceed directly to the claim / protest area. Driver must enter the claim / protest area. Drivers must remain in the claim / protest area until directed otherwise by a track official. If any of the top four finishers do not go directly to the claim / protest area, they will be treated as refusing to exchange / protest.

6.4 WHO CAN CLAIM/PROTEST

Any driver finishing fifth or lower on the lead lap of his/her third consecutive feature may make a claim / protest. Dirt Devils - driver must finish within five positions of the car he/she is claiming and be on the same lap.

Drivers changing cars or provisional drivers can not claim. Pill claim will be enforced the last four points nights of the season and/or as needed. Pill claim means the claimer draws at random from the top four positions to determine the car to claim. In case of multiple claims for the same car, the lowest qualified driver gets priority.

Any driver making a claim / protest must drive his/her racecar, under it's own power, directly to the claim / protest area. Driver can not stop his/her vehicle or consult with anyone else. Claims / protests must be made to a track official with cash upon declaration of intention to claim / protest. Only the driver may request, agree to, or refuse claim / protest and is the only one allowed in the claim / protest area. Driver making claim must complete claim / protest or lose all money and points for the night. Once claim is accepted, claimer and claimer may not restart their engines. Cars will be pushed to the removal area.

First acceptance or refusal by driver being claimed / protested is binding.

6.5 FIRST REFUSAL PENALTIES

Forfeiture of all earnings for the night and loss of points for the year. Driver also forfeits his/her right to claim / tear down for 12 months from date of refusal.

Driver is suspended for 4 events from date of refusal.

6.6 SECOND REFUSAL TO EXCHANGE

Result in the driver being suspended from competition for one year to date.

Any driver found to be claiming / tearing down for someone else will lose all points for the year and be suspended for a minimum of two completed races.

All claimed engines must be removed from the car at the track. Once claim has been accepted the exchange is final.

There is a 1½ hour time limit on engine removal, if not complete in this time, it will be considered a refusal of claim.

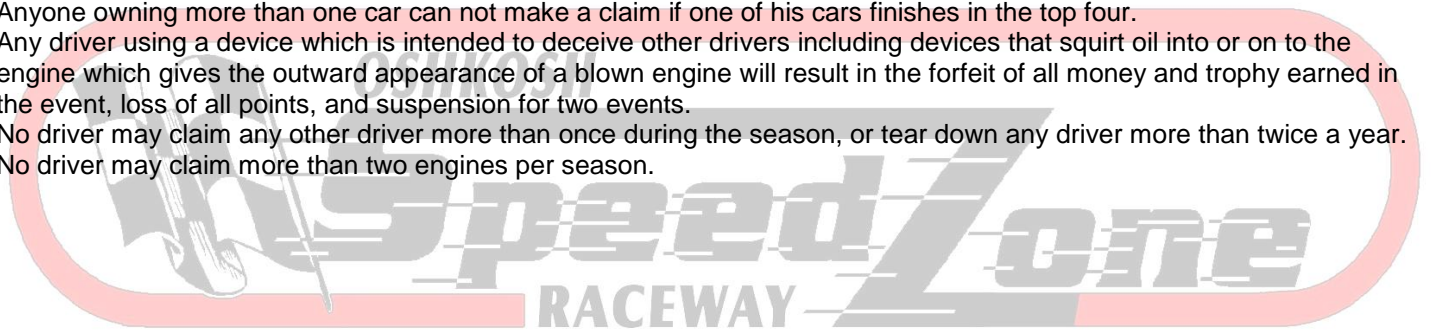
Any sabotage to a claimed engine will result in saboteur's suspension from competition for one year to date.

Anyone owning more than one car can not make a claim if one of his cars finishes in the top four.

Any driver using a device which is intended to deceive other drivers including devices that squirt oil into or on to the engine which gives the outward appearance of a blown engine will result in the forfeit of all money and trophy earned in the event, loss of all points, and suspension for two events.

No driver may claim any other driver more than once during the season, or tear down any driver more than twice a year.

No driver may claim more than two engines per season.



SECTION - 11



SPORT MODIFIED DIVISION 2012

ALL GENERAL TRACK RULES APPLY. PLEASE REFER TO GENERAL TRACK RULES SECTION FOR CAR NUMBER REQUIREMENTS, APPEARANCE GUIDELINES, START TIMES, AGE REQUIREMENTS AND MANY MORE ISSUES. RACE TEAMS NOT COMPLYING WITH GENERAL RULES WILL NOT BE ALLOWED TO COMPETE. RULE VIOLATIONS ARE IN LISTED IN THE GENERAL RULES.

IF IT DOES NOT SAY IT IS ALLOWED IN THE RULES, IT IS ILLEGAL. IF YOU ARE UNSURE OF ITS LEGALITY PLEASE CONTACT A SPEEDZONE OFFICIAL.

THANK YOU!

11.1 SAFETY EQUIPMENT:

Rules apply at all times car is on track. Snell-rated SA2000, SA2005, SFI 31.1/2005 or 2010 helmet required. Roll bar padding required in driver compartment (*Fire retardant recommended*). SFI-approved full fire suit required. Fire retardant gloves, shoes and neck brace (or head and neck restraint) required. Right and left seat head supports required if using head restraint system with no neck collar. *Recommended: Fire retardant head sock and underwear; collapsible steering shaft.* Driver-side window net required, minimum 16 inch by 20 inch ribbon or mesh style, and must be mounted to roll cage so latch is at top front of window. Minimum three inch (two-inch with head restraint system) wide SFI-approved five point safety belt assembly required (Y-type shoulder harness not allowed), must be mounted securely to roll cage, *recommended to be no more than one year old.* Kill switch required within easy reach of driver and must be clearly marked 'OFF' and 'ON'.

11.2 FRAME:

1964 or newer OEM perimeter American rear-wheel drive passenger car frame only. No sports car frames. Frame must be full and complete, cannot be widened or narrowed, and must be able to support roll cage on both sides, exceptions are: weight jack in original center line of spring tower allowed; frame may be cut a maximum 36 inches forward from center of rear end housing; horns may be removed in front of steering box and notched maximum one inch at bottom for tie rod clearance; front crossmember may be notched and boxed for radiator and/or steering clearance; maximum seven inch wide opening in side of spring tower for spring removal. Maximum two inch wide by four inch tall frame stiffener may be welded directly to outside of left side frame rail. Minimum wheelbase 108 inches, maximum 112 inches, both sides. Maximum overall width shall not exceed 78 inches from outside of tire to outside of tire. For cars using OEM rear suspension design, rear of frame behind rear tires no further forward than one inch behind factory seam, may be replaced with two inch by three inch steel tubing with 0.095 inch wall thickness. No part of frame can be lower than four inches from ground except front crossmember.

11.3 ROLL CAGE:

Must consist of continuous hoops, minimum 1.75 inch O.D. tubing, with minimum wall thickness of 0.095 inch for main cage, frame-mounted in at least six places, *low carbon or mild steel recommended*. Must consist of a configuration of front, rear and top hoops connected by tubing on sides or side hoops. Driver's head must not protrude outside cage with helmet on. Roll cage must be securely supported and braced with minimum one cross bar in top halo. Foot protection bar required. Main cage no further forward than rear of engine. All bars forward of cage must be lower than hood.

11.4 DOOR BARS:

All driver side door bars and uprights must be minimum 1.5 inch O.D. with 0.083 inch wall thickness. Minimum three driver side door bars, parallel to ground and perpendicular to driver, and welded to front and rear of roll cage. Passenger side must have at least one cross door bar, horizontal or angled, minimum 1.25 inch O.D. with 0.083 inch wall thickness, and one top door bar, minimum 1.5 inch O.D. with 0.083 inch wall thickness. Steel door plate, 18 gauge or 0.049 inch minimum thickness, must be securely welded to outside of driver side door bars and cover area from top door bar to bottom door bar and from rear hoop down-post to five inches in front of seat. Must be visible for inspection.

11.5 BODY:

(See diagram) No composite or plastic body panels allowed except roof rock guard and hood scoop. Body must be same width, front to rear, and parallel to OEM frame. Aluminum nose panel must be flat. Maximum 2.250 inch side fins allowed on aluminum nose. MD3 plastic nosepiece, part#020-410 allowed. All nose panels may be no wider, or lower than two inches outside of front frame horns and remain within 0.5 inch tolerance outside confines of front bumper. Plastic nosepiece must be mounted in an approved manner and can extend no further back than 6 inches from front of hood. Cooling holes allowed. Engine compartment must remain open (no side panels). Hood must be level or sloped down at front, enclosed and flush with interior deck at rear. No reverse hood rake allowed. No panel in front of right door to engine compartment. No inner panels. No car covers. Must have front and rear roof support posts. Driver and passenger side windows must have at least 12 inch opening (height and width), measured at center of window, between lowest point at top and highest point at bottom. May use lexan in window side panels. Full size, fiberglass or aluminum roof only (see diagram). No dished roofs allowed. Driver roof hatch allowed. Maximum 1.5 inch rolled down rock guard allowed on roof front. Maximum four inch roof sides allowed. Maximum one inch ridge down sides of roof. Maximum one inch rear roof stiffener (must face down). Window side panels must resemble all aspects of drawing and may not extend ahead of back of seat. Rear spoiler (optional) may be minimum one inch to maximum five inches in material height and maximum 66 inches wide. Spoiler may have rear stiffener, must be one inch or more down from top. Maximum three spoiler braces allowed, must be mounted in line. Spoiler braces must resemble all aspects of drawing. Spoiler must be mounted within confines of spoiler braces. No fins, lips or wings allowed. No reflective doors or quarter panels. Tires must be widest part of car. Car number must be minimum four inches thick and 20 inches tall and clearly visible, on both sides, top and back of car, and front, if possible.

11.6 DRIVER COMPARTMENT:

Must have minimum three windshield bars in front of driver. Lexan or aluminum cowl panel in front of driver can be no wider than cockpit and no farther back than steering wheel. Minimum 0.125 inch aluminum, or 0.060 inch steel, complete floor pan required. Aluminum high-back seat only and must be bolted in, using minimum 0.375 inch bolts, next to left side frame rail and ahead of rear tires. Bottom of seat can be no lower than bottom of frame rail. Driver must be sealed off from track, driveline, engine, fuel cell, canisters and pumps. Oil coolers must not protrude above interior. Accumulators cannot be mounted between driver and left-side door bars. No driver-adjustable devices allowed while car is in competition except brake adjuster. No mirrors of any kind.

11.7 FRONT SUSPENSION:

All components must be steel, unaltered OEM, in OEM location, and replaceable by OEM parts, exceptions are: tube-type upper A-frames with or without aluminum or steel cross shaft; and mounts can be moved; OEM replacement stamped steel lower A-frames; rubber, nylon or steel lower A-frame bushings, no offset or bearing type; welded or bolted shock mount on lower A-frame; OEM or OEM replacement ball joints allowed. Lower A-frames must be right and left, and of same design. Lower A-frame mounts and bolt holes on frame must be in OEM location. OEM ball joints only. Sway bar must be unaltered OEM.

11.8 STEERING:

No rack and pinion. All components must be steel, unaltered OEM, in OEM location, exceptions are: outer tie rod end and adjustment sleeve may be replaced by a minimum 0.625 inch steel rod end and steel tube; spindles can be ground for brake caliper clearance only; unaltered, OEM replacement Pinto spindles; bolt on spindle savers allowed; steel steering shafts and knuckles only driver compartment steering may be modified, must be kept on left side. Spindles must be right and left, and of same design. Quick release required - steering quickener and steering wheel may be aluminum. Idler arm, pitman arm, and center link must match frame.

11.9 SHOCKS:

One steel, nonadjustable, unaltered shock per wheel only. All shock mounts must be welded. No shocks allowed on screw jacks. No external or internal bumpers or stops. No coil over, air, or remote reservoir shocks. No Schrader valves or bladder type valve allowed. Front half of any shock may be shielded. One or all shocks may be claimed per event for \$50 each, counting as one claim on card, following shock claim procedures (Refer to Track Operations and Inspection Manual).

11.10 SPRINGS:

One steel coil or multi-leaf (rear) spring per wheel only. Minimum 4.5 inches O.D., non-progressive coils only. No torsion bars, air bags or inner liners.

11.11 REAR SUSPENSION:

All components must be steel. All mounts and brackets must be welded or bolted solid. Coil springs must remain vertical and over center line of rear-end housing. No coil-over eliminators allowed. No chains, cables or tethers. Rear shocks cannot be mounted on control arms. All rear control arms and panhard bars must be straight. Must utilize one of the following designs:

(A) Aftermarket three link design requirements: Must use 16 inch minimum, 24 inch maximum lower control arms. Must use one upper control arm, solid tube only, located at top center of rear end housing and remain centered (1-inch tolerance) on housing over drive shaft. Must use minimum 23 inch panhard bar located behind rear end housing. Lower spring perch must be welded to rear-end housing. Must use steel upper weight jack. No floating or bearing rear spring perches/cups allowed.

(B) Multi-leaf spring design requirements: Must use steel multi-leaf springs with no additional suspension components besides one shock per wheel. Adjustable aluminum lowering blocks allowed.

(C) OEM stock design requirements: Rear crossmember, control arm mounts and bolt holes on frame must be in stock location. All components must be unaltered, approved OEM, and match frame. Control arms cannot be altered in any way. Steel, rubber or nylon control arm bushings only. Springs must remain in stock location. Lower spring perch must be welded to rear-end housing. Must use steel upper weight jack.

11.12 REAR END:

Any steel approved OEM passenger car or truck non-cambered rear end (housing and carrier) allowed, must be centered in chassis. All components must be steel, except lowering blocks, axle cap, and drive flange. Safety hubs (floaters) allowed. No quick change devices. Inspection hole in housing required. Mini-spools only. No torque dividing mini spools or differentials. Ring gear, center section and yoke cannot be lightened. Solid steel axles and one piece drive flanges only.

11.13 BUMPERS:

(See diagram) Steel bumpers must be on front and rear at all times and welded, or mounted with minimum .375 inch bolts.

Rear bumper must be constructed of solid square, or minimum 1.25 inch O.D. tubing with 0.095 wall thickness, and – similar to diagram - no wider than five inches outside of rear frame rails. If wider than five inches outside rear frame rails, must be capped and bent forward 90 degrees, or constructed in a loop design. Must have at least one upright, minimum 1.25 inch with 0.065 wall thickness, from bumper to fuel cell guard. Two-bar front bumper must be minimum 1.25 inch O.D. tubing with minimum 0.065 wall thickness (maximum 0.095 inch) mounted frame-end to frame-end, no wider than width of material outside frame horns and with bottom loop parallel to ground. Top bar must be directly above bottom bar, minimum 6.5 inches apart, measured center to center.

11.14 TIRES/WHEELS:

Must use unaltered Hoosier Race tire, G60-15 with t IMCA stamped on sidewall. No chemical softening, conditioning, or grooving of tires (Refer to Track Operations and Inspection Manual for automatic penalties). Tires may be ground or siped within confines of tread (not past factory straight line). No re-caps. All wheels must be three or four inch backspace and wheel manufacturer decal. No wheel adapters, spacers or bleeder valves. May use Track approved bead lock, on right side only. External, steel bead lock only and it cannot make wheel any narrower than 8 inches and no wider than 8.75 inches. Must use only steel bolts. Foam type or plastic outer mud cover allowed on right side wheels. Inner mud cover allowed on left rear only. Must use minimum one inch O.D. lug nuts.

11.15 BRAKES:

Must be steel approved OEM, operative four wheel, drum or disc. Must maintain minimum OEM dimensions for hubs/rotors and calipers, cannot be lightened. No oil bath front hubs. Bolt pattern may be changed. Larger studs allowed. Rear rotors may be aftermarket 0.81 inch thickness (new). Vented rotors only, no scalloped or ceramic coated rotors. One proportioning device allowed (one-to-one ratio), front to rear only. Brake lines must be visible and must connect directly from master cylinder to calipers with no devices in between. Rear caliper brackets must be welded or bolted solid to rear-end housing.

11.16 EXHAUST:

Round tube headers only. All primary header tubes must enter directly into one collector, at same point, at end of header. Collector length maximum 9 inches. Turn down (maximum 10 inches) allowed. Non-stepped headers only. No heat wrap, or exotic coatings on headers. Schoenfeld mufflers, stamped IMCA 609, must be used if track has noise reduction rule of 98 d.b. or more. All exhaust must go through mufflers, two per car, one per header. No exhaust sensors, merge collectors, cross-overs, extensions, inserts or balance tubes.

11.17 FUEL SYSTEM:

Racing fuel cell required, maximum 32 gallon capacity (*12 gallon recommended*), must be in minimum 20 gauge steel container. Must be securely mounted behind rear axle, between rear tires, minimum four inches ahead of bumper, minimum 10 inches above ground. Must mount with minimum two solid steel straps around entire cell, two inches wide and 0.125 inch thick. All cell mounts must be steel, securely welded to frame/cage. Protective tubing must cover rear and extend past both sides of cell. No part of cell shall be lower than protective tubing. Fuel cell vents, including cap vent, must have check valves. If fuel cell does not have aircraft style positive seal filler neck/cap system - a flapper, spring or ball type filler rollover valve is required. Pick-up must be on top or right side of cell. One fuel filter allowed. No cool cans. Air cleaner top/stud cannot direct air into carburetor. No top flow air cleaner housings. Mechanical OEM type push rod fuel pumps only. Maximum 0.100 inch thick carburetor gaskets on all engines. CLAIM ENGINE: must use SPEED ZONE approved naturally aspirated, unaltered 500 c.f.m. Holley - part no. 0-4412, may be modified to Holley HP Dorton part no. 0-80583-1 specs only. Float bowl must face forward. Any adapter, maximum one inch thick. No throttle bore adjustable carburetor spacers. GM CRATE ENGINE: may use any Holley 4 barrel carburetor, all components (float bowls and main body) must be Holley manufactured. Metering blocks and base plate may be billet aluminum non-Holley. No aerosol-style carburetors allowed. May use Speedway Motors part #545-64940 or Moroso part #64940 carburetor spacer on crate. All carburetors, may be exchanged, style for style only, following same procedure as engine claim. Driver claiming carburetor may not claim engine or shocks on same night.

11.18 FUEL:

Gasoline only. Racing fuel allowed. No E85. *Pump grade recommended*. No performance enhancing or scented additives. Fuel must pass both dielectric meter and chemical tests. Fuel sample may be taken from any car at any time (Refer to Track Operations and Inspection Manual for automatic penalties).

11.19 WEIGHT:

Minimum weight limit of 2,500 pounds, no tolerance, after race with driver in car. Weights must not be used in driver compartment or outside body. All weights must be securely mounted with at least two 0.5 inch bolts, painted white with car number on it. No titanium, magnesium, stainless steel or carbon fiber components. Solid steel fasteners only.

11.20 BATTERY/STARTER:

One 12 volt battery only, must be securely mounted between frame rails, and positive terminal must be covered. Starter must bolt on block in OEM location and directly engage flexplate/flywheel. Car must have capability of starting without being pushed or pulled. Car must leave initial staging area on demand, unaided, or go to rear of that race.

11.21 GAUGES/ELECTRONICS:

No unapproved cameras, transmitting or listening devices (exception is one-way RACEceiver radio by officials), timing retard controls, or digital gauges (including tach). No electronic monitoring computer devices capable of storing or transmitting information except memory recall analog tach. 12 volt ignition system and OEM HEI distributor only. Ford/Chrysler may use HEI distributor. No billet distributors or crank triggers. Ignition rotor, cap, coil and module must remain OEM appearing. Crate engine MUST use MSD #8728 rev-control and 6,200 rpm chip. Claim engine MAY use MSD #8728 rev-control with any rpm chip. Rev-control must be out of reach of driver, but accessible for inspection with rev limiter facing upward. No ignition boxes, remote coil or accessories. All wiring must be visible for inspection. Only gauges allowed are analog oil pressure, fuel pressure, brake bias, water temperature and tach. OEM type alternator with internal regulator allowed. No electronic traction control devices.

11.22 TRANSMISSION/DRIVE SHAFT:

All forward and reverse gears must be operational, plus a neutral position. With engine running and car in still position, driver must be able to engage car in gear and move forward, then backward. Only OEM production transmissions allowed. No 'in and out' boxes or quick change devices allowed. Functioning shift levers must be in OEM location. One steel or aluminum OEM style/size flywheel or steel OEM style/size flexplate allowed, must be bolted directly to end of crankshaft.

Automatic: Must be unaltered, two or three speed, OEM production case with a functioning OEM appearing pump. Aluminum OEM bellhousing may be replaced with aftermarket explosion-proof aluminum bellhousing. Original OEM bellhousing must have approved scattershield constructed of minimum 0.125 inch by three inch steel, 270 degrees around flexplate. Only external lines allowed are for transmission cooler. Splined drive flange coupler or torque converter (10 inch minimum) only.

Manual: Must be unaltered, three or four speed, OEM production case and have a working 7.25 inch minimum diameter, steel and/or aluminum, single or multi-disc clutch and pressure plate bolted directly to flywheel/flexplate. These components must rotate, consistent with engine rpm, while car is in any gear. Must use explosion-proof steel bellhousing with one hole for throw out bearing lever or hose, must be 270 degrees around top of clutch and flywheel/flexplate area. Hydraulic clutch pedal allowed with manual transmission only.

Drive Shaft: Minimum two inch diameter, white, steel drive shaft. Steel slip-yokes only. 360-degree drive shaft loop required and must be constructed of at least 0.25 inch by two inch steel, or one inch tubing, mounted six inches back from front U-joint.

11.23 ENGINE COMPARTMENT:

Rear of engine (bellhousing flange) must be mounted at least 72 inches forward from centerline of rear axle. Engine offset must be kept within two inches of centerline of front crossmember with engine level. Minimum 11 inch engine height from ground to center of crankshaft. V-belt aluminum or steel pulleys only. Copper/brass or aluminum radiator only and must be mounted in front of engine. No vacuum pumps or pan evac systems, oil coolers or remote oil filters.

11.24 ENGINE OPTIONS AND SPECIFICATIONS:

All cars must clearly display on driver side front roof post which engine they are competing with. Must be contrasting in color from body, minimum 2-inches tall and display CLAIM or CRATE. Markers not acceptable.

(A) CRATE ENGINE: Must use unaltered sealed GM #88958602 or #19258602 crate engine. Upon inspection, any different, altered or missing GM seal bolts will result in disqualification, loss of all Track points for the season, \$1,000 fine and a one year suspension from all divisions with crate option. GM seal bolt exception is Track approved and issued cable-lock rebuild system. \$250 fine and two week suspension for any crate engine not using required spacer, distributor, rev limiter, pushrods, valve springs or rocker arms. Any driver using crate engine cannot claim engine or have engine claimed. During same season, no driver is allowed to claim an engine after competing with a crate. If a driver switches to a crate after claiming an engine, the crate engine is then claimable.

(B) CLAIM ENGINE: All engines must be able to be used in conventional passenger car without alterations. External engine casting and threaded holes cannot be altered.

BLOCK: OEM steel passenger vehicle production block only. No GM Bowtie, Ford SVO or Chrysler W components allowed. GM approved block numbers are: 3892657, 3914660, 3914678, 3932388, 3932386, 3956618, 3970000, 3970006, 3970010, 3970014, 10066033, 10066036, 10243880, 14010207, 14010209, 14010287, 14016376, 14016379, 10054727, 14088528, 14088548, 14088552, 14093638, 14101148. Stroke must match block. No 400 or larger cubic inch parts allowed. Maximum 361 cubic inches (GM); 363 (Ford); 370 (Chrysler). Violation of cubic inch limit must be verified by removal of head and will result in disqualification, loss of all Track points for the season, \$1,000 fine and a 30-day suspension. Maximum compression ratio is 9.0 to 1, no tolerance. Compression ratio checked using Whistler and cubic inches checked using pump, OR by visual inspection of part and/or casting numbers, pistons, etc (track option which method is used). Flat top or dished pistons only, no gas-ported pistons. OEM or OEM replacement steel crankshaft only - cannot be lightened (no aerowing, bullnose, knife edge or undercut allowed. Effective January 1, 2011, no drilling of second or third rod throws). OEM or OEM replacement steel rods only – GM 5.7 or 6-inch rod allowed. Cap screw allowed. Conventional flat tappet cam and lifters only, cannot alter lifter bores. OEM firing order cannot be changed (GM: 1-8-4-3-6-5-7-2). May use oil restrictors. 'Wet' sump oiling system only. Steel oil pans only. Racing oil pans allowed. Mandatory one inch inspection hole in all pans – no obstructions to crank and rods. Accumulator allowed.

CYLINDER HEADS: Steel only. Must be unaltered approved OEM and minimum 76 cc combustion chamber (GM). Only GM OEM approved head numbers are: 14079267, 3986336, 3986339, 3986339X, 3986388, 3932441, 376445, 3928454, 3932454, 3876487, 3973487, 3973487X, 3973493, 3951598, 468642, 330862, 333882, 3998920, 3998991, 3998993, 3998997, 3970126. Maximum size valves on these heads are 2.02 inch intake and 1.60 inch exhaust. May use Engine Quest (EQ) Stock Replacement (SR) cylinder head, part number CH350I, head must remain as produced, seat angles and valve sizes can not be changed: three angle valve job only (absolutely no casting removal in valve pocket of EQ head, for any reason); Ford - no after market or SVO heads; Chrysler - no after market or W-2 heads, 360 cubic inch heads only. No porting, polishing or unapproved alterations allowed to any cylinder head or intake, disqualification and \$250 fine if illegal. Guide plates, screw-in shouldered studs (GM 0.375-inch max) and polylocks allowed. No stud girdles. Steel roller tip rocker arms allowed. GM - 1.250 inch maximum O.D. valve springs, no beehive valve springs allowed.

INTAKE: Unaltered, approved OEM cast iron low rise, two- or four-barrel. Only unaltered (no porting or polishing) aftermarket aluminum intakes allowed are: Weiland GM #7547-1; Ford #7515, #8023 or #7516; Chrysler #7545; Edelbrock GM #2701; Ford #7121, #7181, #7183; Chrysler #2915. Cooling lines allowed on aluminum intakes. Disqualification, loss of points, purse and \$250 fine if any alterations are found to heads/intake. Unaltered OEM type harmonic balancer only. OEM type steel or aluminum water pumps only.

11.25 ENGINE CLAIMING RULES:

Refer to Track Operations and Inspection Manual for claim eligibility requirements.

(A) \$550 cash claim on engine, \$25 goes to wrecker for pulling engine and \$25 goes to official.

(B) Claim does not include - 1. flywheel, 2. clutch, 3. pressure plate, 4. bellhousing, 5. breathers, 6. carburetor, 7. starter, 8. motor mounts, 9. oil/temp. sending units, 10. fan and pulleys, 11. clutch ball, 12. clutch arm, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor, 18. plug wires, 19. water outlet and restrictor, 20. headers.

NOTE: During engine claim, block and head numbers should be verified, heads, intake, connecting rods and crankshaft should be visually inspected and one spark plug must be removed to check flat top or dish pistons prior to transfer of engine to claiming driver. If, at this time, engine is found illegal, claimed driver is disqualified and suspended from all events until such time as a \$250 fine is paid, for first offense. Claimer then has option to accept or decline engine - if declined, driver is not charged with claim. Second offense penalties will be \$500 fine and/or suspension.

11.26 POINT STRUCTURE /PROCEDURES:

Refer to Track Operations and Inspection Manual.

11.27 EIRI:

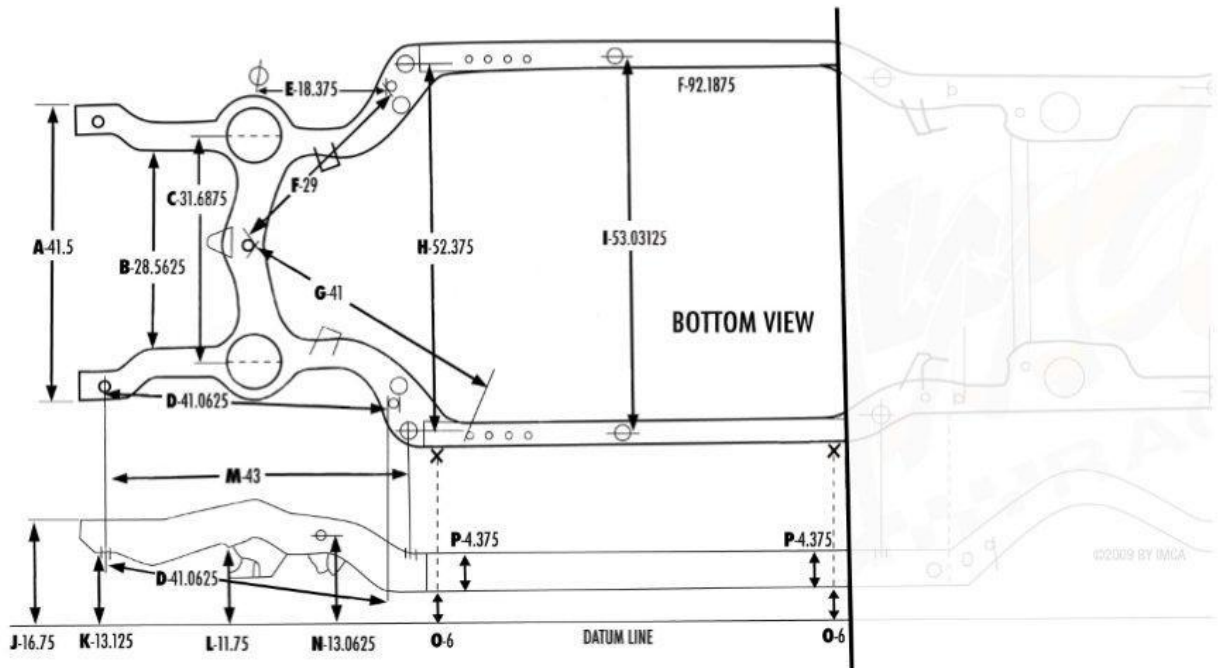
(Except in rare instances) Decisions of officials are final and binding without exception.

2011 GM Chevelle '64-'72 Frame Dimensions

- Dimensions are to either the top or bottom surface of the frame rail as indicated.

- Some dimensions may not apply as frame can be trimmed for front bumper.

- All dimensions must be within 0.250-inch tolerance. Exception is variances officials verify as crash damage.

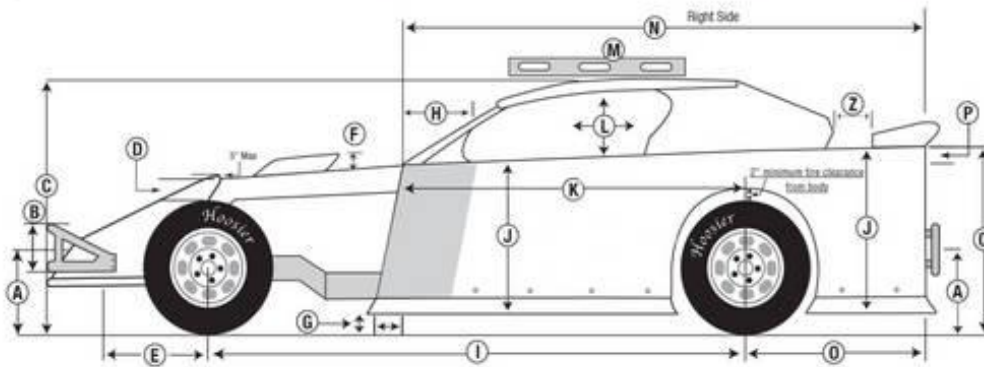


- A.** 41.5 - inches to outside of frame rail
- B.** 28.5625 - inches between side rails at lower steering gear bolt and idler arm mounting surface.
- C.** 31.6875 - inches TRAM top side between upper control arm inner shaft seats (at shim contact area).
- D.** 41.0625 - inches from center of hole (frame horn bottom), to front edge of hole.

- E.** 18.375 - inches from center of lower ball joint grease fitting, to front edge of hole. Maximum difference of 0.5 inches from side to side of frame.
- F.** 29 - inches from rear edge of hole, to front edge of hole.
- G.** 41 - inches from rear edge of hole, to front edge of hole.
- H.** 52.375 - inches from center of hole to center of hole.
- I.** 53.03125 - inches from center of hole to center of hole.

- J.** 16.75 - inches to top surface of side rail.
- K.** 13.125 - inches to bottom surface of side rail.
- L.** 11.75 - inches from bottom surface of side rail (just forward of coil spring opening) to datum line.
- M.** 43 - inches measured from center of hole to center of hole.
- N.** 13.0625 - inches center of hole to datum line.
- O.** Locations for measuring ride height. Dimensions J, K, L and N should be measured at 6 inch ride height.
- P.** 4.375 - inches from top to bottom of side rail surface.

Northern SportMod Body Dimensions For 2012



All body measurements are max, unless otherwise specified. Dimensions have zero tolerance.

- A** 20" max, 16" min. (ground to center of bumpers, front and rear)
- B** 6.5" min. (center to center)
- C** 54" max, 42" min.
- D** Hood 6" max. drop (sides), sealed off from driver's compartment and max. 3° rake.
- E** 42" max.
- F** 6" max.
- G** 4" min. Door may be extended maximum 6" past block at bottom on both sides.
- H** 19" max., must be same on both sides.
- I** 112" max, 108" min.
- J** 31" max, 22" min.
- K** 72" max, or not past back of block, right side. Left side may extend forward to cover foot box.
- L** 18" max, 12" opening, both sides.
- M** With level, must have no more than 2" clearance at rear of roof and 5" at front.
- N** 120" max, 106" min.
- O** 48" max, 34" min.
- P** Interior slope is 6" max, front to rear and flat across. If flat at front half of interior you have used up 50% of your 6" so from behind driver to rear you may only have 3". Top of interior must be flush with top of doors and quarters. Optional escape hatch from driver to passenger door allowed.
- Q** 38" max, 28" min.
- R** Min. 24" nose must be flat and flush with sides.
- S** 66" max, 24" min, must cover radiator.
- T** 52" max, 44" min.
- U** 56" max, 41" min.
- V** All lines must be outside body and nerbar and visible from front, rear & top.
- W** 66" max, 53" min, same front to back
- X** 43" max, 24" min.
- Y** IMCA decal required on filler panel.
- Z** 3" minimum
- REAR FILLER PANEL:** 8" min. / 90 degree angle. Must be solid, attached to deck, extend to quarter panels, securely fastened.
- SPOILER BRACES:** Maximum 3 total. Bottom edge of spoiler brace must be flush with rear edge of interior deck.
- SAIL PANELS:** Must be within confines of these dimensions and the same on both sides. Must mount within 2" of outside edge of interior deck.

