



WISCONSIN'S PREMIER 1/3 MILE CLAY RACE TRACK

460 E. County Rd Y
OSHKOSH, WI 54901



2012

OSHKOSH SPEEDZONE RACEWAY RULE BOOK

Official interpretation of these rules is made by Track Officials and is final. Amendments to these rules may be made at any time at the discretion of the Track Officials. Non-compliance may result in not being able to compete, loss of points, loss of monies, fines, and/or suspensions.

RULES DISCLAIMER

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules. **NO EXPRESS OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM PUBLICATIONS OF OR COMPLIANCE WITH THESE RULES AND/OR REGULATIONS.** They are intended as a guide for conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, officials, or others. The race director shall be empowered to permit minor deviation from any of the specifications herein or impose any further restrictions that in his/her opinion does not alter the minimum acceptable requirements. **NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM SUCH ALTERATION OF SPECIFICATIONS.** Any interpretation or deviation of these rules is left to the discretion of the officials. Their decision is final.

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**IMPACT MOTORSPORTS LLC
D.B.A. OSHKOSH SPEEDZONE RACEWAY
PO BOX 3226
OSHKOSH, WI 54903-3226**

DAY OF EVENT, TRACK NUMBER - 1-920-279-9096 / 920-252-1463
Website - www.oshkoshspeedzone.net
Email – oshkoshspeedzone@yahoo.com

REPRESENTATIVES:

Competition Director –

Modified Rep. – Dennis Baldry – 920-203-0296 (Cell)

Grand National Rep. – Jerry Winkler – 920-203-6455 (Cell)

Street Stock Rep. – Louis Reichenberger – 920-426-0523 (Home)

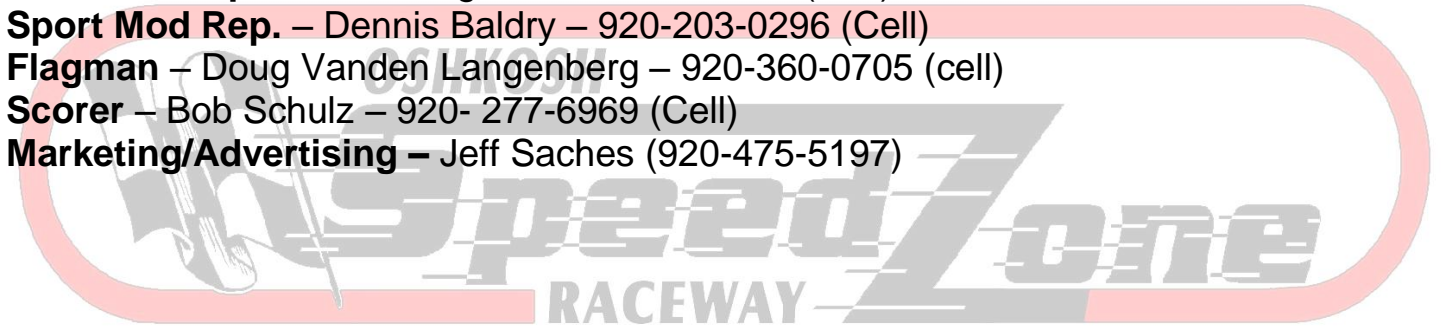
Dirt Devil Rep. – Mike Morgan – 920-427-8373 (Cell)

Sport Mod Rep. – Dennis Baldry – 920-203-0296 (Cell)

Flagman – Doug Vanden Langenberg – 920-360-0705 (cell)

Scorer – Bob Schulz – 920- 277-6969 (Cell)

Marketing/Advertising – Jeff Saches (920-475-5197)



IMPACT MOTORSPORTS LLC – OWNERS

JEFF LEMIESZ – 920-279-9096 (Cell)

LARRY STRATTON – 920-252-1463 (Cell)

REVISIONS/CLARIFICATIONS UNDERLINED

**DIAGRAMS IN THIS BOOKLET ARE FOR REFERENCE ONLY WITH THE
EXCEPTION OF THE MODIFIEDS**

SECTION - 1

2011 OSHKOSH SPEEDZONE RACEWAY OPERATION RULES

1.1 SCHEDULE OF EVENTS

4:00 – Pit gates open.

6:00 – Race car registration deadline for lineups or you will be considered late.

6:00 – Mandatory Drivers Meeting. If not present you will be put in the back of your heat. Roll call may be done.

6:15 – Track Packing if needed – If you do not pack when asked you will start all events in the back row.

6:25 – Opening Ceremonies; National Anthem with parade lap of previous weeks feature winners.

6:30 – Racing starts.

1.2 ANNUAL RELEASE FORM

An annual release form must be filled out prior to admittance to the pit area. Annual release card must be shown prior to admittance to the pit area. If you do not have an annual release form filled out, you will be asked to fill one out and sign the waiver.

1.3 PIT AREA

Anyone under 12 years of age must be accompanied by a parent or guardian to enter the pit area. Anyone under 18 years of age must have an insurance minor release form filled out, signed and on file with Impact Motorsports LLC before admission.

Wrist band must be worn on left wrist at all times. Drivers must purchase and wear a wrist band for each division that they are competing in. If you are caught in pit area without wrist band you will be fined \$50.00 plus suspended from pit area until fine is paid.

Racecar driver is the sole spokesman for the car in any and all matters pertaining to an Event, and is responsible for the actions of the pit crew, and owner.

No alcoholic beverages may be opened in the pit area until after the entire program is completed – any violation will result in being ejected from the track, loss of points and/or earnings may also be forfeited.

Proper footwear required in the pit area.

1.3.1 ATV, GOLF CARTS & MOTORCYCLES

ATV's & Golf Carts will be allowed; ATV's and Golf Carts must register with the Speedzone and show proof of insurance. Registration forms will be available online and through Impact Motorsports LLC.

Motorcycles and bicycles are prohibited from the pit area, unless prior arraignments have been made or allowed by touring series.

1.3.2 PIT STALLS

Front row pit stalls can be reserved for \$100 for the season (including specials). Prior rented stalls will be available to previous year's renters until the end of January of the upcoming year then on a first come first paid basis. Any other spot may be reserved (including specials) for \$50. You must supply your own pit stall sign designating your reserved spot.

1.3.3 WASTE PRODUCTS

No dumping petroleum products, anti-freeze, tires, etc. on track premises – any violation may result in fines, loss of points, and/or loss of monies. Any competitor that spills antifreeze, or any non-approved chemical, on the track or in the pit area, will be held responsible for all investigation and clean up costs.

1.4 DRIVERS

Drivers must be at least 14 years of age to drive in the 4 cylinder division.

Drivers must be at least 16 years of age to compete in all other divisions. Drivers younger than 16 wishing to compete in other divisions must be approved by track Officials.

Any driver under 18 years of age must have an insurance minor release form filled out, signed and on file with Impact Motorsports LLC before admission. Drivers must purchase and wear a wrist band for each division in which they are competing.

Drivers are considered independent contractors and assume responsibility for all charges, premiums and taxes, if any, payable on any funds received as a result of participation in any events. Impact Motorsports LLC and sponsors reserve the right to use photos of any driver or their car for promotional purposes.

Drivers must wear full face helmet and seatbelts whenever racecar is on the track. The only exception is during the National Anthem.

1.5 TRACK PACKING

All cars in all divisions are required to assist in packing the track when needed. When a division is called to pack the track, all cars in the pit area must report to the track. Any cars that do not assist in packing the track will start all races in the last row of their race for the entire Event. Alternate vehicles may be used for track packing, but must be registered with the Speedzone. One track packing vehicle per car. Packing vehicle must utilize the same tires as the division and display the car number. Track packing vehicles kept at the track must park in designated area and leave key. **ALL vehicles must be in running order!**

1.6 MUFFLERS

All cars must be equipped with a muffler. Cars cannot exceed a noise level decibel reading of 95 measured at 100 feet from the outside of track. Any car with noise levels that contribute to a violation of the community noise limit: loss of points and money for Event; and/or accumulated points for current year.

1.7 CAR NUMBERS

All numbers will be registered with Impact Motorsports. All numbers must have a letter. The letter must be at least A **6" OR TALLER**. All numbers on the doors must be a minimum of 20" tall and 3" wide. A **6" OR TALLER NUMBER REQUIRED ON FRONT AND BACK OF CAR**. Numbers must be in a contrasting color. No metallic numbers. Numbers must be kept clean and to be between 00 and 99. Failure to meet these requirements you risk the chance of not being scored or allowed to compete. A number is required on the roof, readable from the right side, and must be as large as possible. IT IS THE RESPONSIBILITY OF THE DRIVER TO ENSURE THAT THE SCORERS CAN READ THE CAR NUMBER. IF YOUR CAR NUMBER IS NOT READABLE FROM THE SCORING TOWER, THE CAR WILL NOT BE SCORED UNTIL THE NUMBER IS CORRECTED.

Track Officials reserve the right to assign or restrict the display of graphics and advertising on race cars. Offensive graphics or slogans are not permitted. All Competitors agree to accept Track's decision in this matter.

Where required, participating sponsors emblems, decals, or Track technical stickers, will be placed in the position designated by Track Officials. Cars that do not display all sponsors emblems, or decals, will receive less prize money.

1.8 BALLAST WEIGHT

Ballast weights must be painted white with car number clearly marked on it.

1.8.1 IF WEIGHTS FALL OFF CAR

a) FIRST OFFENSE:

Car will be black flagged for that event, plus a \$1.00 dollar per pound lost fine.

b) SECOND OFFENSE:

Car will be disqualified for that event plus a \$2.00 dollar per pound lost fine.

1.8.2 DRIVE SHAFT

Drive shaft must be painted white and car number on it.

1.9 DIAGRAMS

Diagrams published in this rule book are intended for reference guidelines only unless otherwise noted. The only exception to this rule will be in accordance of the Modified division. The Modified division will continue to follow the rules as written in this rule book.

1.10 DRIVERS MEETING

All drivers, or a representative from each car, must attend the drivers meeting. Attendance may be taken. If the driver, or car representative, is not present, the car will start all events in the rear that the car qualified for. Each car will be exempt from this rule its first night at the track to accommodate newcomers.

1.11 TRANSPONDERS

Transponders are required on all race cars. It is the driver's responsibility to make sure the transponder is in the correct placement and secured. If a transponder falls off during competition, the penalties are as follows:

a) First offense

Heat race - black flag and put in „B" feature.

Feature - black flag and scored in last place with that place points and money

b) Second offense

Heat race and/or feature - black flag and loss of points and money for night.

c) Third offense

Suspension and fine, discretion of track promoter and Track Officials.

If transponder stops working you will be scored until verification of missing or broken transponder is clarified. Practices will be used to test and record transponder locations. IT IS THE RESPONSIBILITY OF THE DRIVER TO ENSURE THAT THE TRANSPONDER IS FUNCTIONING PROPERLY. IF YOUR TRANSPONDER IS NOT FUNCTIONING PROPERLY, THE CAR WILL NOT BE SCORED UNTIL THE PROBLEM IS CORRECTED.

1.11.1 TRANSPONDER LOCATIONS

1.11.1.1 Modifieds, Grand Nationals, Street Stocks and Sport Modifieds must be placed twenty four inches (24"), or more, behind the center line of the rear axle to the forward edge of the transponder, on the right side frame rail.

1.11.1.2 Dirt Devils are to be located in the spare tire well area recommended 12", or more, behind the center line of the rear axle to the forward edge of the transponder.

Note: No metal of any kind can be between transponder and track. Transponder pouch should be placed horizontally with no obstructions between the pouch and the ground. If transponder pouch is mounted to round tube frame components with nylon ties, secure the transponder pouch so that it cannot spin from its horizontal position. This may be done by duct taping the nylon ties to the frame rail.

All transponder locations must have a clear view of track for signal to transmit.

1.12 PIT CLOSING TIME

The pit area will be closed at 12:00 am, (Midnight). Failure to clear the pit area within this period will result in the loss of prize money, and/or points. The amount of prize money, and/or points that will be lost is the decision of the Competition Director, and/or track management, which is final. All drivers are responsible for keeping their pit stalls clean. Track management reserves the option to assess pit stall cleanup costs for failure to keep pit stalls clean and use proper waste disposal containers.

SECTION – 2 SAFETY

2.1 TRACK SAFETY

Racing is an inherently dangerous sport. Each Competitor assumes the risk of injury or death when he/she participates in an Event. Competitors are solely, and directly, responsible for the safety of their race cars and racing equipment. OSHKOSH SPEEDZONE RACEWAY IS NOT RESPONSIBLE FOR THE ADEQUACY OF A COMPETITORS RACE CAR OR RACING EQUIPMENT. NO EXPRESS, OR IMPLIED, WARRANTY SHALL RESULT FROM THE PUBLICATION OF, OR COMPLIANCE, WITH THESE RULES. These rules govern the conduct of an Event, and, by participating, Competitors are deemed to be in compliance with these rules.

2.2 SAFETY EQUIPMENT

A full face, helmet and face shield, Snell-rated SA2000, SFI 31.1/2005, SA2005 or SA2010 required or manufactured build date for no longer than a 10 year time period and MUST be in good condition. Helmet and face shield must be worn at all times car is on the track and must accompany vehicle at time of inspection. Roll bar padding required in driver compartment (*Fire retardant recommended*).

2.2.1 REQUIRED

SFI-approved full fire suit, fire retardant gloves, shoes, driver-side window net, minimum 16 inch by 20 inch ribbon or mesh style, and must be mounted to roll cage so latch is at top front of window.

Seat belt and shoulder harness recommended being date stamped, 2010, 2011 or 2012 and being in good condition. (Y-type shoulder harness prohibited.) Belts must not be frayed. Seat belt and shoulder harness must be installed according to manufacturer's recommendations. Lap and shoulder belts must be approved snap-type release belts at least three (3) inches wide and securely fastened to the frame and or roll cage. A crotch or anti-submarine belt is mandatory, and must be a minimum two (2) inches wide.

2.2.2 RECOMMENDED

Neck brace or head and neck restraint. Right and left seat head supports required if using head restraint system with no neck collar. Fire retardant head sock and underwear; collapsible steering shaft recommended. . IF NO NECK BRACE IS USED A FIRE RETARDANT HEAD SOCK MUST BE USED!

2.3 DISCONNECT SWITCH

A kill switch required within easy reach of driver and must be clearly marked „OFF“ and „ON“.

A second battery disconnect switch, must be mounted behind or next to driver on deck cover and within reach of the safety crew. The disconnect switch must shut off all power to the car and be brightly colored. The battery must be securely installed. The positive terminal must be covered.

2.4 HEAD CLEARANCE

All divisions must adhere to the 2 inch head clearance rule!

Measurements will be taken from top of helmet to top of top roll bar with driver strapped in race ready!

SECTION - 3 PENALTIES

3.1 PROCEDURE

If a Raceway Technical Official observes, or is made aware of, a violation of the Raceway Rules, by a Competitor, Raceway Officials will investigate claim and can impose an appropriate penalty if found to be true.

3.2 EMERGENCY ACTION

If an act by a Competitor is determined by Raceway Officials, Raceway Technical Officials, or persons serving under Raceway direction, to threaten the orderly conduct of an Event, the Raceway Officials can take emergency action against the Competitor. Such action may include: arrest; ejection from the racing premises; suspension from competition; or any other action to remove the threat created by the Competitor. Examples of conduct warranting emergency action include, but are not limited to: consumption of alcoholic beverages in pit area before or during an Event; use of illegal drugs before or during an Event; harassment, verbal abuse, or assault of any Raceway Officials, Raceway Technical Official, or Competitor; fighting; reckless driving; and failure to obey a black flag or directions of a Raceway Official. The emergency action will remain in effect for the period of time determined by the Raceway Officials, except for an ejection which is final and non-appealable.

3.3 PAYMENT of FINES

Fines must be paid to Oshkosh Speedzone Raceway and will be deposited in the current discretionary fund. Failure to pay fines may result in suspension from competition. All unpaid fines may be collected, by Oshkosh Speedzone, by deducting the amount from the purse or point fund. If the Competitor is not a driver, the fine may be deducted from the purse or point fund of the driver with whom the competitor is associated at the time of the violation. Any unpaid fines remaining at the end of the racing season will be carried over to the next racing season and be deducted from the purse or point fund until all monies are collected.

3.4 SCOPE of PENALTIES

Penalties for violation of Raceway Rules are determined by the severity of the violation. Penalties include, but are not limited to: fines; loss of points; loss of purse; disqualification; suspension of driving privileges. Oshkosh Speedzone Raceway will use the following guidelines for determining penalties. **A greater or lesser penalty may be imposed depending on the circumstances.**

3.5 GENERAL

- a) Any Competitor who performs an act or participates in an act deemed by Track Officials as detrimental to auto racing or the Track: a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- b) Any Competitor who signs the release sheet for anyone else: ejection.
- c) Any parent or legal guardian that falsifies an insurance waiver for a minor Competitor: ejection of the minor Competitor; and/or suspension of the minor Competitor; and/or loss of points and money for Event; and/or loss of accumulated points for current year; and a fine determined by Track Officials.
- d) Any Competitor who harasses, verbally abuses, or assaults any Track Official, Technical Official, or persons serving under Track direction: ejection; a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- e) Any Competitor who participates in fights on the racing premises: ejection; a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- f) Any Competitor who, while participating in a Speedzone Event, consumes any alcoholic beverages and/or illegal drugs, or is under the influence of alcohol and/or illegal drugs: a fine determined by Track Officials; ejection & disqualification; and suspension for a period of time determined by Track Officials.
- g) Any Competitor who stops on the track to argue with a Track Official: a fine determined by Track Officials; and/or suspension; and/or loss of points and money for Event; and/or loss of accumulated points for current year.
- h) Any Competitor who fails to correct an unreadable car number: stop scoring of the car until the number is corrected; and/or a fine determined by Track Officials.
- i) Any driver not wearing a full driver's suit and/or gloves and/or other mandated driver safety equipment during an event: "disciplinary" black flag; and/or a fine of \$25.00.
- j) Any Competitor who leaves tires in the pit area or anywhere on the track property: a fine of \$25.00/tire.

3.6 ROUGH DRIVING

- a) Disciplinary black flag, fines, suspensions and loss of points based on severity of incident is at the discretion of track officials.
- b) Intentional use of vehicle in a malicious manner - minimum \$1000 fine and/or suspension is at the discretion of track officials.

3.7 FUEL

Sample may be taken from any car at anytime.

- a) First violation - \$250 fine.
- b) Second violation - 3 event suspension plus the cost of inspection fees. Only completed Events are counted towards suspension.

SECTION - 4

TRACK PROCEDURES

4.1 RACE DECISIONS

All decisions by Oshkosh Speedzone Raceway Officials involving track procedures are final, and non-appealable.

4.2 RAIN OUTS

Raceway Officials will consult with the track owner to determine if the Event will be postponed.

In the event of a rain out prior to the opening of the pit gate, information can be obtained by calling the race track. The track phone number for "Oshkosh Speedzone Raceway" is **(920) 252-1463 / (920) 279-9096**.

In the event of a rain out after the pit gates are open, all qualifying races and B Main races must be completed in order to receive points and purse. Completed races will receive full points and purse. Uncompleted A Main races will receive total points and purse for the race, divided equally among the race cars. Rain out passes, if issued, or wrist bands from the event, are good only for the next two (2) completed events at the track where the rainout occurred.

4.3 WEIGH IN

All drivers are encouraged to weigh their race car, prior to racing. Any race car under the allowable weight is subject to disciplinary action. Penalties will be imposed in accordance with **SECTION 3 - PENALTIES**. Random weight checks may be conducted at any time. Weights will be determined by the track scale, which is considered official. The top five race finishers in all divisions, or as designated, must report to the Claim/Inspection Area immediately after the race. Winners may go into victory lane without penalty then to scaling.

4.4 LINEUPS

It is the driver's responsibility to make sure your car is **REGISTERED AND SIGNED IN** and car number is on the lineup sheet. Failure to do so will result in being placed at the rear of the heat lineups.

All cars must be lined up in the staging area in the proper position, when the front row cars begin to move out of the staging area. Cars entering the staging area or race field after the front row cars begin to move must start the race at the rear in the order they join the field. Exceptions will be made for drivers competing in more than one division per night, or a Competitor who notifies a pit steward of extenuating circumstances.

4.5 CAR AND DRIVER CHANGES

A driver may drive a different car that has been qualified but will start the event from the rear. If a driver qualifies for the feature but needs to use an unqualified car to compete, the driver must race through the "B" main to qualify for the feature. The driver will start in the last place position of the "B" main.

ALL DRIVER AND CAR CHANGES MUST NOTIFY ONLY THE COMPETITION DIRECTOR, COMPETITION DIRECTOR WILL NOTIFY SCORING TOWER OF CHANGES!

4.5.1 DRIVER SUBSTITUTION

A driver may have a substitute driver once a year. Original driver must notify registration crew one week in advance. Registration crew will make note of the change.

4.6 CONDUCT

The driver is the sole spokesperson for the car in any and all matters pertaining to an Event, and is responsible for the actions of the pit crew, and owner. Determination of crew affiliation will not be limited to the pit sign in sheet. It will be a determination made by the track staff. Harassment or abuse of Raceway Officials will not be tolerated and is subject to disciplinary action.

4.7 INJURIES

All injuries must be reported to a Raceway Official prior to leaving the race premises. Track insurance will not cover any unreported injuries. Any driver involved in a roll over, or severe accident, must be checked and cleared by the safety staff prior to returning to the track.

4.8 CAMERAS

In car cameras will be allowed for personal use only and are thus deemed unofficial. Unofficial cameras will not be used to change or determine any calls made before, during, or after a race event.

4.9 SAFETY VEHICLES

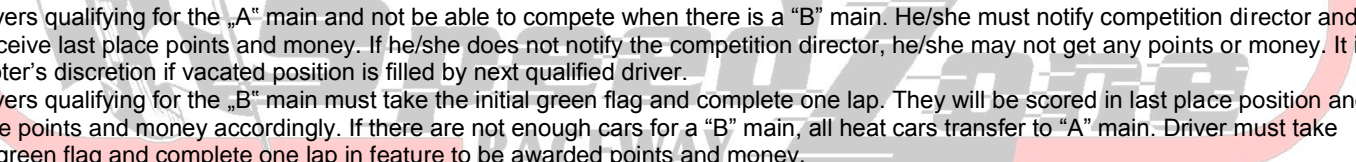
Any driver that hits any track vehicle will pay for damages!

SECTION - 5 POINTS/PAYOUT PROCEDURES

5.1 POINTS

Weekly „A“ feature win is 40 points with each subsequent position worth one point less, with 24th position in feature worth 17 points. If track runs a „B“ feature(s), first non-transferring driver receives 16 points and each subsequent position, back to sixth position, receives one less point per position. Drivers that do not qualify for „A“ or „B“ feature will not receive points or money. Heat points are awarded. 1st - 3 points; 2nd - 2 points; 3rd - 1 point.

1. 40	Transfer	Heat 1 - 3
2. 39	Transfer	Heat 2 - 2
3. 38	Transfer	Heat 3 - 1
4. 37	Transfer	
5. 36	16	
6. 35	15	
7. 34	14	
8. 33	13	
9. 32	12	
10. 31	11	
11. 30	11	
12. 29	11	
13. 28	11	
14. 27	11	
15. 26	11	
16. 25		
17. 24		
18. 23		
19. 22		
20. 21		

- 
- a) Drivers qualifying for the „A“ main and not be able to compete when there is a „B“ main. He/she must notify competition director and will receive last place points and money. If he/she does not notify the competition director, he/she may not get any points or money. It is promoter's discretion if vacated position is filled by next qualified driver.
- b) Drivers qualifying for the „B“ main must take the initial green flag and complete one lap. They will be scored in last place position and receive points and money accordingly. If there are not enough cars for a „B“ main, all heat cars transfer to „A“ main. Driver must take initial green flag and complete one lap in feature to be awarded points and money.
- c) Points remain with the driver, not with the car.
- d) Divisions that have less than ten entries, one point fewer than normal is awarded for each car less than ten. For example, if nine cars entered, feature win would then be worth 39 points; for eight entries, win would be worth 38 points, etc.
- e) Points will determine track championships. In case of a tie, feature wins will be counted, then seconds, thirds, etc.

5.2 PAYOUT

All positions in features get money and/or a pass. „B“ mains receive money if the initial green flag and one lap is taken. Heat races do not pay – they are for points and qualifying for „B“ and „A“ features. Earnings are paid out at the following event from 4:00 to 6:00 in pole shed. Any unclaimed earnings will be available the following week during the same time period. Any driver wishing his check mailed must provide a stamped self addressed envelope (SSAE). Any money not claimed after **30** days will be returned to Impact Motorsports LLC.

Driver must compete in seventy percent (70%) of scheduled events and attend awards banquet to be eligible for point fund.

5.2 LINE-UP PROCEDURES

Heat races will be lined up according to the starting position drawn by the driver, for the first Event only. After the first Event, qualifying races will be lined up according to average Speedzone driver points, with the lowest point average drivers to the front and the highest point average drivers to the rear. Drivers without Track points will be placed in the last row. If a driver is absent for two consecutive events (excluding rainouts), they will be treated as a new driver the first week they return. The deadline for race car registration and lineup is 6:00 PM, or you will be considered late and placed in the last row.

Point averages are = (total points) / (number of events competed in).

For the first night of racing all drivers draw two numbers. The first number is for heat starting position. The second number is the redraw for „A“ feature starting positions. One heat - 5 redraw; two heats - 10 redraw; three, four, five, or six heats - 12 redraw.

„B“ features are lined up from heats with highest finishers at the front.

‘A’ features: Heat qualifiers are lined up by inverted point average. When one heat is run the top 5 qualifiers from the heat are inverted. When two heats are run the top 10 qualifiers will be inverted. When three, four, five, or six heats are run the top 12 qualifiers will be inverted. The remaining heat qualifiers are lined up behind inverted field as they finished. A driver that qualifies for the invert but has no point average shall be lined up in the last invert position. Should there be more than one driver with no point average; they will be lined up at discretion of officials. Impact Motorsports LLC also reserves the right to relocate a driver to the rear of the field if deemed necessary for the safety and well-being of all involved. Remainder of „A“ feature is lined straight up from heats and/or „B“ features. If a driver is absent for two consecutive completed events they are treated as a new car (zero point average) for the first week he/she returns. Total points are not lost.

5.4 LAPS

Heats - 1 lap per car with a maximum of 10 laps, or 1 minute per lap.

„B“ features will normally be 1 lap per car with a maximum of 15 laps, or 1 minute per lap.

„A“ features will normally be 20 laps.

Track officials have the option to alter race length based on car count, weather, time, etc. Race will be scored complete after laps are completed or time limit is up.

5.5 RACING

Cars must enter the track from turn three and leave the track using the turn two exit.

On original starts the front two cars are double file and pace the start together with moderate speed. These front row cars are given 2 chances at a clean start before both cars being sent to the rear of the starting field. No jackrabbit starts allowed – flagman’s discretion. Any car(s) causing a yellow flag before the initial start of the race will restart the race in the last row.

All restarts will be double file, except for the leader who will be alone in the first row. The car leading the race controls the restart. A restart cone will be placed at the exit of Turn 4. After the “one lap to go” signal is given, and/or the drivers are notified over the Raceciever, all cars must stay nose to tail. Lagging back is prohibited. On all restarts, all cars must maintain their position and may not accelerate until the lead car passes the restart cone. Any cars lagging back prior to the restart, hitting the cone, or passing to the left of the restart cone, will be penalized two positions on the next caution. Any cars passing prior to the lead car passing the restart cone will be penalized two positions per position advanced on the next caution. If no caution occurs, the driver will be penalized at the end of the race. Any car(s) causing a yellow flag before the restart of the race will restart the race in the last row. Single file restarts will be at the discretion of the flagman.

Any single driver spinning out unassisted and causing one (1) yellow flag in a heat race, or two (2) yellow flags in the „B“ feature or „A“ feature race, will receive a disciplinary black flag. This does not apply to a driver that spins out to avoid a wreck. A driver that spins out to avoid a wreck and does not make contact with the wall or any other vehicle may receive their position back. The determination of whether a driver spins to avoid a wreck is a judgment call by the flagman, which is final.

Driver must use pit lane if he/she wishes to re-enter the race. Any car on pit lane will rejoin the field from the rear. Driver must pull over to the right as much as possible while on pit lane. If a car pulls off the track in turn three the car is considered in the pit and out of the race.

If a driver enters pit lane for repairs, repaired part must be checked by pit lane person staged at the end of the pit lane. If driver fails to stop, he/she will be black flagged and not allowed to re-enter event. Lapped cars will start race at back of field.

No one but the driver is allowed on the track at any time during an event. Driver will be disqualified from the event if pit crews enter the track unless specified by track officials. Driver will lose all points and pay for that event.

No unapproved communication, recording, or traction control devices will be allowed in any car.

A SW1600 model Raceciever, tuned to channel 0000 (454.000) is mandatory for all divisions.

5.6 STARTS AND RESTARTS

On ALL restarts, drivers should line up single file front to back on caution laps to give scoring time to put all cars in correct position.

Once positions are correct, drivers will be notified via Receiver to form two wide with leader in front by him/herself. On all new starts, two wide front to back. In the event the two front cars cannot make pace, at the flag mans discretion, the two cars may be sent to the back of the field and the row will just move ahead. On all starts and restarts, at the flag mans discretion, if the start cannot be accomplished, he may make a single file start. We will use the acceleration cones in the apex of corner 3 & 4 with the official starting cone at the end of corner 4.

5.7 FLAGS

5.7.1 GREEN FLAG

The green flag, and/or green light, indicates the start of the race or the restart of the race after a caution period. The green flag and green light will be displayed as the lead car(s) pass by the flagman. All race starts will be double file, nose to tail. ("Nose to tail" means staying in the tire tracks of the car ahead of you.)

5.7.2 WHITE FLAG

The white flag indicates that there is one lap remaining. If a yellow flag occurs after the white flag is displayed and the leader has crossed the start/finish line, the race is considered complete. All cars that have passed the start/finish line will be scored as they crossed the start/finish line. All remaining cars, **except** those involved in the incident, will be scored according to their last completed lap. The cars involved in the incident will be scored at the end of their last completed lap.

5.7.3 CHECKERED FLAG

The checkered flag indicates that the race is complete. Once the checkered flag is displayed to the race leader, the race is considered complete. All cars receiving this flag must slow to a reasonable speed and, with the exception of the winner, return to the pit area. Failure to slow to a "reasonable speed" may result in disqualification. The determination of reasonable speed is a judgment call by Track Officials, which is final. If the yellow flag occurs after the checkered flag is displayed to the race leader, all cars that have passed the start/finish line will be scored as they crossed the start/finish line. All remaining cars, **except** those involved in the incident, will be scored according to their last completed lap. The cars involved in the incident will be scored at the end of their last completed lap. Race winners will report to the "winner's circle" for post race ceremonies. Finishing positions will be determined by the number of laps completed, whether the car is running, or not. Effective Date: January 1, 2010

5.7.4 YELLOW FLAG

The yellow flag, and/or yellow light, indicates caution on the track. All cars receiving this flag, and/or light, must slow, hold their position, and form a single file line behind the lead car. **Absolutely no racing back to the yellow flag.** The penalty for racing back to the yellow flag, as determined by the flagman, is restarting the race at the rear. If a pace vehicle is used, the leader must line up behind it. Cars will be lined up as they were scored on the last completed lap. Lapped cars will be placed in the back of the field. Any cars entering pit lane during the caution period must restart the race at the rear, in the order they return to the track. Re-entry under the yellow flag is not permitted until the lineup is complete. If the yellow flag occurs before the completion of one lap, the cars will be lined up in their original starting order **except** those cars involved in the incident, which must restart the race at the rear. Any driver, as determined by the flagman, intentionally causing a yellow flag (such as spinning out another car, stopping on the track, or capable of resuming the race after a spin but does not) will receive either a one lap penalty or "disciplinary" black flag. No car may pass the pace vehicle unless directed by a Raceway Official. Any car illegally passing the pace vehicle is subject to the black flag. Prior to restarting the race, the flagman will signal one lap to go.

5.7.5 BLUE FLAG WITH DIAGONAL YELLOW STRIPE (FASTER TRAFFIC APPROACHING)

The blue flag with the diagonal yellow stripe indicates that faster traffic is approaching. Cars receiving this flag must prepare to yield to faster traffic. Cars should hold their line on the track and allow the faster traffic to pass. If the slower cars are running side by side, they must form a single line at the bottom of the race track. Failure to obey a "faster traffic approaching" flag is subject to a "disciplinary" black flag.

5.7.6 RED FLAG

The red flag, and/or red light, indicates that the race must stop immediately, regardless of the position of the cars on the track. During a red flag, pit crew members are not allowed on the track. All car repairs must be done on pit lane. If the red flag occurs before the completion of one lap, the cars will be lined up in their original starting order **except** those cars involved in the incident, which must restart the race at the rear. If the red flag occurs after one complete lap, the cars will be lined up in single or double file according to the last completed lap. Prior to restarting the race, the flagman will signal one lap to go.

5.7.7 BLACK FLAG

Pull off the track in turn two onto pit lane and Pit Steward will advise you the reason for the flag. If black flag is for a mechanical problem, car may return to track after making necessary repairs. If black flag is for disciplinary reasons, the car and driver are disqualified from race.

SECTION - 6 INSPECTIONS

6.1 INSPECTIONS / ENGINE EXCHANGE / PURCHASE / PROTEST / SEALING

Track officials have the right to inspect any engine or car at any time.

The Competitor will be given a "reasonable amount of time" to remove components from the car for post race inspection or claim. The Competitor will be notified as to what is considered a "reasonable amount of time" at the beginning of the inspection or claim. If the removal of the components is not completed in this time period, the car will be disqualified.

The cars crew and or driver will perform the actual teardown or engine removal with their own tools and equipment. Only two (2) of the cars crew and the driver will be allowed in the Inspection/Claim area.

The individual(s) representing the car during post race inspection or claim may not consume any alcoholic beverages, or be under the influence of alcohol, until the post race inspection of the car or claim is complete.

The top five race finishers in all divisions, or as designated, must report to the Inspection/Claim Area immediately after the race.

6.2 ALL CLAIMS, EXCHANGES AND PROTESTS WILL BE SUBJECT TO A \$50.00 ADMINISTRATION FEE.

6.2.1 MODIFIEDS

ENGINE CLAIMING RULES:

6.2.1.1 \$1,050 cash, or \$100 and exchange, claim on engine, flywheel and balancing plates (\$25 goes to wrecker and \$25 to official for each engine). Claimed driver has option of accepting cash, or exchanging engines with claiming driver.

6.2.1.2 Claim does not include - 1. Clutch, 2. Pressure plate, 3. Bellhousing, 4. Headers, 5. Carburetor, 6. Starter, 7. Motor mounts, 8. Oil/temp. Sending units, 9. Carburetor spacer, 10. Fan and pulleys, 11. Clutch ball, 12. Clutch arm, 13. Throw out bearing, 14. Dip stick, 15. Water pump, 16. Fuel pump, rod and plate, 17. Distributor, 18. Plug wires, 19. Water outlet and restrictor, 20. Breathers.

6.2.2 STREET STOCKS

Track officials have the right to purchase an engine for \$750.

Protest only, \$200 top, \$300 bottom, \$500 for complete engine.

6.2.3 GRAND NATIONALS

Protest only, \$200 top, \$300 bottom, \$500 for complete engine.

6.2.4 DIRT DEVILS

\$600 claim swap for entire car except for the seat, seat belts, window net and fuel cell (if equipped). Track officials reserve the right to buy any car for **\$1000** less the seat, seat belts, window net and fuel cell (if equipped).

6.2.5 SPORT MOD

ENGINE CLAIMING RULES

6.2.5.1 \$550 cash claim on engine, \$25 goes to wrecker for pulling engine and \$25 goes to official.

6.2.5.2 Claim does not include - 1. flywheel, 2. clutch, 3. pressure plate, 4. bellhousing, 5. breathers, 6. carburetor, 7. starter, 8. motor mounts, 9. oil/temp. sending units, 10. fan and pulleys, 11. clutch ball, 12. clutch arm, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor, 18. plug wires, 19. water outlet and restrictor, 20. headers.

NOTE: During engine claim, block and head numbers should be verified, heads, intake, connecting rods and crankshaft should be visually inspected and one spark plug must be removed to check flat top or dish pistons prior to transfer of engine to claiming driver. If, at this time, engine is found illegal, claimed driver is disqualified and suspended from all events until such time as a \$250 fine is paid, for first offense. Claimer then has option to accept or decline engine - if declined, driver is not charged with claim. Second offense penalties will be \$500 fine and/or suspension.

6.3 WHO CAN BE CLAIMED/PROTESTED

Top four finishers of the „A“ Feature whether running or not are subject to engine exchange claim / protested. The top four drivers must proceed directly to the claim / protest area. Driver must enter the claim / protest area. Drivers must remain in the claim / protest area until directed otherwise by a track official. If any of the top four finishers do not go directly to the claim / protest area, they will be treated as refusing to exchange / protest.

6.4 WHO CAN CLAIM/PROTEST

Any driver finishing fifth or lower on the lead lap of his/her third consecutive feature may make a claim / protest. Dirt Devils - driver must finish within five positions of the car he/she is claiming and be on the same lap.

Drivers changing cars or provisional drivers can not claim. Pill claim will be enforced the last four points nights of the season and/or as needed. Pill claim means the claimer draws at random from the top four positions to determine the car to claim. In case of multiple claims for the same car, the lowest qualified driver gets priority.

Any driver making a claim / protest must drive his/her racecar, under it's own power, directly to the claim / protest area. Driver can not stop his/her vehicle or consult with anyone else. Claims / protests must be made to a track official with cash upon declaration of intention to claim / protest. Only the driver may request, agree to, or refuse claim / protest and is the only one allowed in the claim / protest area. Driver making claim must complete claim / protest or lose all money and points for the night. Once claim is accepted, claimee and claimer may not restart their engines. Cars will be pushed to the removal area.

First acceptance or refusal by driver being claimed / protested is binding.

6.5 FIRST REFUSAL PENALTIES

Forfeiture of all earnings for the night and loss of points for the year. Driver also forfeits his/her right to claim / tear down for 12 months from date of refusal.

Driver is suspended for 4 events from date of refusal.

6.6 SECOND REFUSAL TO EXCHANGE

Result in the driver being suspended from competition for one year to date.

Any driver found to be claiming / tearing down for someone else will lose all points for the year and be suspended for a minimum of two completed races.

All claimed engines must be removed from the car at the track. Once claim has been accepted the exchange is final.

There is a 1½ hour time limit on engine removal, if not complete in this time, it will be considered a refusal of claim.

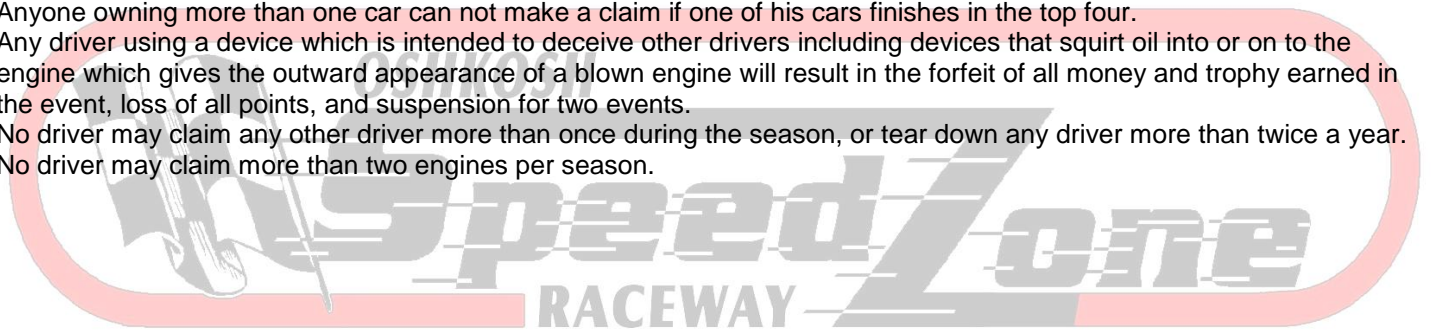
Any sabotage to a claimed engine will result in saboteur's suspension from competition for one year to date.

Anyone owning more than one car can not make a claim if one of his cars finishes in the top four.

Any driver using a device which is intended to deceive other drivers including devices that squirt oil into or on to the engine which gives the outward appearance of a blown engine will result in the forfeit of all money and trophy earned in the event, loss of all points, and suspension for two events.

No driver may claim any other driver more than once during the season, or tear down any driver more than twice a year.

No driver may claim more than two engines per season.



SECTION - 9 STREET STOCK DIVISION 2012

ALL GENERAL TRACK RULES APPLY. PLEASE REFER TO GENERAL TRACK RULES SECTION FOR CAR NUMBER REQUIREMENTS, APPEARANCE GUIDELINES, START TIMES, AGE REQUIREMENTS AND MANY MORE ISSUES. RACE TEAMS NOT COMPLYING WITH GENERAL RULES WILL NOT BE ALLOWED TO COMPETE. RULE VIOLATIONS ARE IN LISTED IN THE GENERAL RULES.

IF IT DOES NOT SAY IT IS ALLOWED IN THE RULES, IT IS ILLEGAL. IF YOU ARE UNSURE OF ITS LEGALITY PLEASE CONTACT A SPEEDZONE OFFICIAL.

THANK YOU!

9.1 FRAMES

1964 or newer U.S. manufactured rear wheel drive passenger cars full frame or unibody, minimum 108 inch wheelbase. **Camaro, Firebirds, El Camino's, convertible's, and station wagons are prohibited.** Other models are approved provided they are the same body configuration, meet the spirit and intent of competitive racing, and must be approved by Track Officials. All frames or unibodys must remain stock unaltered. X-Bracing, Plating for strength or repair is allowed. The front and rear frame sections of unibody race cars will be connected by a **minimum** of 2"x 3"x 0.12" square tubing, safely welded to maintain a solid floor structure. Rear frame replacement is allowed for rusted rear frame rails. 2"x 4" square tubing must be used in place of rear frame rails, placed no further back than the rear spring pockets and no longer than factory specs.

9.2 Weight

The specific minimum weights for all cars are listed below. All weights are with the driver, after competition.

<u>Car Type</u>	<u>Minimum Weight</u>
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Spec Motor	3100 lb.
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Non Spec Motors	3300 lb.
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Prior to competing all drivers will post the cars weight on the passenger side "A" pillar with decal.

9.3 Weight Penalties

FIRST OFFENSE - disqualification and loss of points and money for that event.

SECOND OFFENSE - disqualification, loss of money for that event, loss of points for year, and a 3 event suspension

THIRD OFFENSE - disqualification, loss of money for that event, loss of points for year, and suspension for remainder of the year

Open shows, and cancelled events do not count as "events" for the purposes of serving suspensions.

9.4 Weight and Ballast

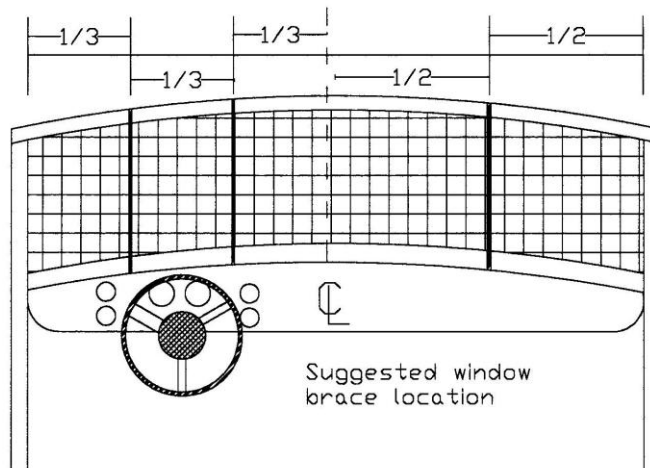
Added weight (ballast) may be steel or lead. All weights must be securely attached for inspection. Loose weights are prohibited.

Added weights must be painted white and have car number on each piece. Weight must be secured by no less than two ½-inch diameter bolts. The maximum spacing between bolts is 10 inches. No more than 50 pounds of weight for every two bolts. No weights may be added outside the body or inside driver's compartment. Weights added behind the rear end shall be no lower than the bottom of the frame member to which the weight is attached.

Loss of ballast penalty see track operations rules.

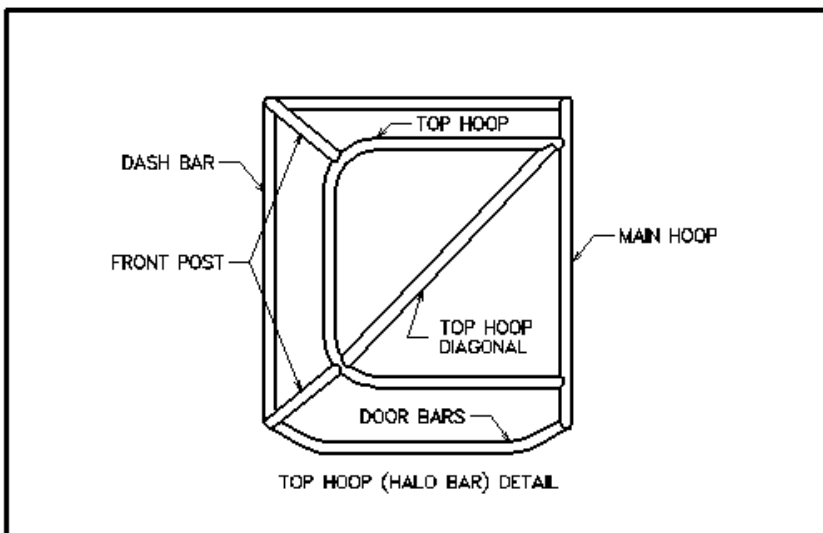
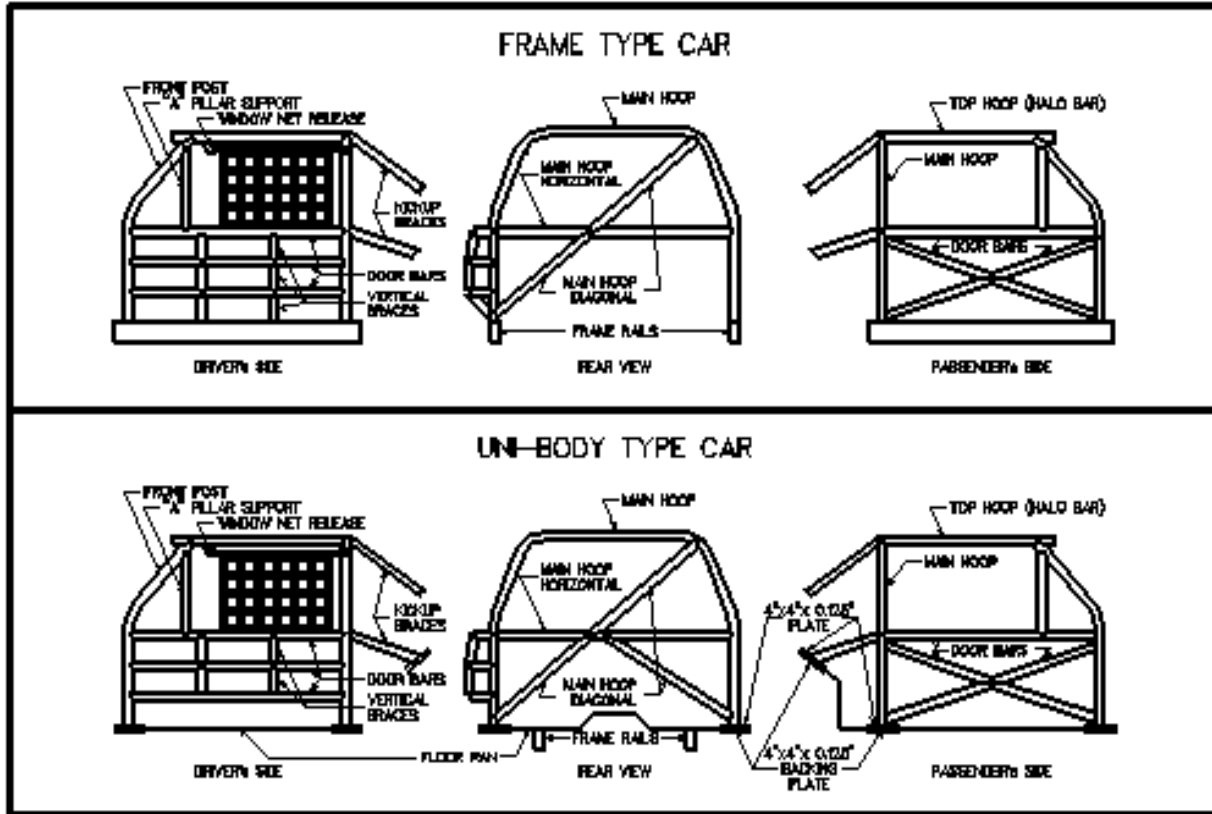
9.5 BODIES

All cars must maintain a neat appearance. Cars must have stock appearing bodies with O.E.M steel or O.E.M steel replacement panels. Body swaps up to the year of 2000, are allowed as long as body matches wheelbase of the frame. Fabricated body components are allowed as long as they have the same steel thickness, wheel openings and contours of the O.E.M. components. Rear spoilers are allowed maximum of 5" and no wider than rear trunk lid. Wheel openings may be cut for clearance but must be capped or rolled edge. All body panels may be gutted, including hood, roof, fenders, doors, quarters and trunk. Hood must be separate from fenders in O.E.M. location, with rear sealed off from drivers compartment with metal. All inner wheel wells may be removed. Rear edge of trunk may be trimmed or removed only if aftermarket tail piece is used. Trunk must be closed off from driver's compartment. All holes covered by rust or equipment removal must be covered by steel. Stock dashboard must be removed and replaced with a non reflective material. All glass (windows, headlights, etc.) Exterior body moldings (chrome trim, door handles, etc.) combustible materials (interior, seats, headliners, etc.) must be removed. Opera windows may be closed off by steel, or lexan. All factory trunk and hood latches must be removed and replaced w/ clip style hood pin assembly. Minimum 4 hood, and 2 trunk pins required. Front and rear bumpers must mount in stock location. Cars using a plastic nose or tail piece may use 1 3/4" round tubing securely fastened to the frame and no wider than the factory bumper. A reinforced, 1" or smaller, full width, steel wire mesh windshield screen is required. The wire diameter shall be no less than .063" for mesh larger than 1/2" or no less than .035" for mesh smaller than a 1/2". A minimum of 3 reinforcements must be installed behind the wire mesh. The reinforcements may be a minimum of 3/8" round, 1/2" square tubing, or 1/2" "T" section. 2 of the reinforcements must be evenly spaced on the drivers half of the windshield opening and one reinforcement must be centered on the passenger's half of the windshield opening. (SEE DIAGRAM) The reinforcements must be bolted or welded to the roof panel or roll bar and dash panel in an approved manner. Rub Rails are allowed and can be no larger than 1" in diameter and have maximum wall thickness of .125" and have the ends cut at a 45 degree angle and be capped and be flush mounted to the body. The rub rail must be mounted between the center of the wheel and top of the rim and be no longer than 4" from each wheel opening. No hood scoops! Rocker panels can not be lower than 5" from ground. Stock interior floor must remain in place
The full stock front firewall must remain in place and any holes must be patched and sealed. If firewall needs to be replaced, the stock dimensions must be used to rebuild firewall.
Floor must remain intact, if your floor is badly rusted or has been removed, you must patch or replace it in a factory appearing manner with 20 gauge (.0359") thick steel plate. Right side of floor may be no higher than the drive shaft tunnel for muffler clearance. (Drive shaft tunnel does not mean transmission bell housing)
Rear firewall maybe located no further forward than rear halo supports, and no higher than bottom of rear opera window. All holes must be covered with min of 22 gage steel.



9.6 ROLL CAGE

Roll cage must be acceptable to track Officials. All bends in the roll bar tubing must have a smooth radius and no kinks. It is mandatory that all joints be gusseted. Offset roll cages are prohibited. Laid-back roll cages are prohibited. The roll cage must be a four-post design consisting, in general, of: a vertical main hoop; top hoop or halo bar with diagonal bar; and left and right front post.



Some tracks may allow alternative rule for metric frames, I.M.C.A. stock car rule [maximum 41" {48" for 1988 1996 GM bodies} from top center of windshield to the front edge of the rear hoop]. Cars running by the I.M.C.A. rule must be preapproved by each track they compete at.

The main hoop must connect to the left and right frame rails, behind the driver, and be diagonally braced. The main hoop may be located no further back than the rear body mount by the frame kick-up. On GM "X" body frames, the back of the main hoop may be located no further back than 82½-inches from the center of the front lower ball joint. The main hoop must have a horizontal bar at the midpoint. The main hoop bar must be round steel tubing no less than 1¾-inches in diameter and have a minimum wall thickness of 0.095 inches. All other main hoop support bars may be round steel tubing no less than 1¾ - inches in diameter and have a minimum wall thickness of 0.095 inches. DOM tubing recommended. Black pipe, exhaust tubing, formed pipe, etc. is prohibited. The top hoop must attach to the main hoop, and left and right front posts. The top hoop must be diagonally braced. A horizontal "dash" bar must connect the left and right front posts. The top hoop, and left and right front posts must be round steel tubing no less than 1¾- inches in diameter and have a minimum wall thickness of 0.095 inches. The dash bar may be round steel tubing no less than 1¾ - inches in diameter and have a minimum wall thickness of 0.095 inches. "A" pillar supports are allowed. The driver's side front post must be connected to the main hoop by three, or more, equally spaced, horizontal bars, mounted flush with the outer door skin. The door bars must be connected by two, or more, equally spaced vertical braces and must attach to the main frame by two, or more, equally spaced vertical braces. A foot protector bar is mandatory. All driver side door bars and braces must be round steel tubing no less than 1¾- inches in diameter and have a minimum wall thickness of 0.095 inches. The complete driver's side door bar area must be plated with steel plates no less than 0.095 inches thick. The passenger side must be equipped with a minimum of three door bars, two of the bars must be "X" design. Horizontal bars must be equally spaced and connected by two, or more, equally spaced vertical braces. All passenger side door bars and braces may be round steel tubing no less than 1¾ - inches in diameter and have a minimum wall thickness of 0.083 inches. The main hoop must be connected to the back of frame kick up by a minimum of two bars. The bars must be round steel tubing no less than 1¾ - inches in diameter and have a minimum wall thickness of 0.095 inches. All roll bars exposed to the driver, and left side door bars, must be padded. All steel bars in roll cage area must be a minimum 1¾ - inches diameter. Chassis' must be equipped with a fuel cell protector bar. The fuel cell protector bar must stay within the confines of the trunk, ahead of the rear bumper, and no lower than the bottom of the fuel cell.

All roll cages must provide 2-inches, or more, clearance, measured from the bottom of the top halo bar of the roll cage to the top of the driver's helmet, when the driver is seated and strapped in, with the helmet on and in the driving position.

9.7 SUSPENSION

9.7.1 Front Suspension

All components and mounts must be steel, unaltered O.E.M, in O.E.M location and match frame. Rubber, polyurethane or nylon lower A-frame bushings only. No offset or bearing type. O.E.M or O.E.M replacement ball joints allowed. For 1978-1987 G.M. mid-size metric frame, O.E.M. Upper A-frame may be replaced using aftermarket upper A-frame. ALL "A" Frames used must meet OEM Specifications. Steel Coleman hub is approved for right front only. Sway bars are allowed as long as in stock location and OEM year make and model and NOT hollow, stock brackets and rubber, or poly urethane bushings. No adjustable cross shafts and no slotting allowed, if slotted upper control arms are used you MUST weld a washer on the slot as not to be able to move control arm. NO Bushing Type, Bearing Type, Heim Joint Type upper control arms allowed!

9.7.2 Steering

All components must be steel, unaltered O.E.M. in O.E.M. location and match frame. Exceptions are: bolt on spindle savers are allowed, O.E.M. steering column must be replaced with steel knuckles and steel steering shaft,(collapsible recommended), quick release is required.

9.7.3 Shocks

Must run steel non adjustable shocks in stock location! ALL SHOCKS must be "STOCK" type shocks!

9.7.4 Springs

Must run steel springs in stock location only. Racing rear spring mounts can be installed on rear axle housing. Non-adjustable spring shims are allowed. Leaf spring cars are allowed up to a five inch (5") spring shackle mounted in stock location and must be equal length side to side.

9.7.5 Rear Suspension

All components and mounts must be steel, unaltered O.E.M., in O.E.M. location. Rubber or polyurethane bushings allowed. **All trailing arms, and mounts must be O.E.M. and in O.E.M. location. Trailing arms can be boxed for strength** or replaced with DCA after market trailing arms. NO adjustments on any trailing arms!

9.7.6 Rear End

Any O.E.M. steel unaltered non-cambered rearend (housing & carrier) and matches frame is allowed. **No factory or aftermarket posi-traction or limited slip carriers allowed!** Mini spool or welded carriers only. O.E.M. or steel aftermarket axels allowed.(No gun-drilled axels) C-clip eliminator kits allowed.

9.7.7 Optional FORD 9"

Stock rear end for frame used or optional Ford 9". Any combination okay as long as parts are steel. No aluminum except drive flanges. Drive flanges to be of equal distance on both sides of frame. No independent rear end (Corvette, Jaguar). Rear trailing arms must remain mounted in stock frame locations. One hole upper and lower for each trailing arm mount location. No aftermarket limited-slip or ratchet style rear ends (Ex: Gold Track, True Track, or Gleason, etc....). Mini spool or welded spider gears only.

Ford 9" allowed, but must be mounted like OEM rear end (centered) for that make and model. One inch inspection hole in housing required. Ring gear, center section and yoke cannot be lightened. Must be welded spider gears, or mini spool. No torque dividing mini spools or differentials. Center of rear lower control arm bolt hole must be 2.25 to 2.5 inches from bottom of housing

Ford 9" and stock rear ends may run rear disk brakes with a minimum rotor thickness of .810 inches.

Other stock rear ends maybe reinforced from backing plate to center housing and also may be XXX'd.

9.8 Transmission

O.E.M. automatic transmissions that match car make.(G.M. in a G.M. or Ford in a Ford etc.). Vehicle must have two forward gears and one reverse, plus neutral. With engine running car in still position must be able to engage car in forward and reverse gears. Torque converter must be unaltered O.E.M. or O.E.M. replacement (No dummy converters allowed). Torque converter must be a minimum of 10" at face. NO add on pumps to transmission. External transmission oil coolers are allowed. No transmission coolers allowed in driver's compartment. Drive shaft must be O.E.M. steel, 2.5 inches in O.D., painted white with labeled with car number. All cars must be equipped with driveshaft hoop. The hoop must be constructed with .25 inch by 2 inch steel located 6 to 8 inches behind the front u-joint connected to the frame.

9.10 BRAKES

Steel, unaltered O.E.M., or unaltered O.E.M. replacement, operative 4-wheel brakes allowed. Front components must match frame and maintain minimum O.E.M. dimensions for hubs, rotors, calipers. O.E.M. diameter caliper pistons only. No vented, scalloped, or ceramic coated rotors. No floating brakes allowed. No brake shut off devices allowed. No adjustable proportion valves. Brake lines must be visible. (Not running thru tubing or frame) Plastic brake lines allowed. You may remove O.E.M. vacuum brake booster and master cylinder and replace with O.E.M. manual brake master cylinder. No after market break pedals with dual cylinders.

9.11 ENGINES- **ALL ENGINES ARE SUBJECT TO INSPECTION AND /OR TEAR DOWN**

ALL ENGINES MUST MEET ALL SPEEDZONE MOTOR SPECIFICATIONS, NO CLAIMER MOTORS.

Only V-8 engines are permitted. Engine may be unsealed or sealed. The unsealed engine is not claimable, but protest able. The sealed engine is not protest able as long as the seals remain intact. Engine must be of Parent Corporation of frame. The maximum displacement for General Motors and Ford engines is 363.0 cubic inches. The maximum displacement for Mopar engines is 368.0 cubic inches. The maximum compression ratio is 10.80 to 1 for all engines. Engine mounts may be after market. The engine must remain in the stock V8 location. No setback allowed. On a GM metric chassis, the centerline of the fuel pump must be no less than 2¾ - inches ahead of an unaltered crossmember.

9.11.1 Engine Blocks

Block must be a factory production cast iron block with external measurements identical to the standard production engine. Angle milling of block is prohibited. All engine block markings must remain.

9.11.2 Crankshaft and Harmonic Balancer

Only OEM cast iron or forged steel crankshafts, or the following aftermarket crankshafts, are permitted:

<u>Manufacturer</u>	<u>Pro-Line Scat</u>	<u>C.A.T.</u>	<u>Eagle</u>
GM	-----	5-350-3480-5700 -----	435034805700
FORD 351W	-----	4-351W-3500-6000 -----	435135006200

Titanium crankshafts are prohibited. Lightweight, knife-edge, and undercut counterweight crankshafts are prohibited. Crankshafts with journals undercut more than 0.030 inches prohibited. Minimum crankshaft weight is 50.5 lbs. Other aftermarket crankshafts may be permitted provided the journal diameter and stroke are the same as OEM and they weigh a minimum of 50.5 pounds. Only OEM, all steel, standard type harmonic balancers are permitted. Aluminum or fluid type balancers are prohibited.

9.11.3 Pistons and Rods

Any after market piston, with the wrist pin in the stock location, may be used. Stock dimension wrist pins must be used. Only OEM magnetic steel, forged, connecting rods, or the following OEM replacement connecting rods, for the make and model engine, are permitted:

<u>Manufacturer</u>	<u>Pro-Line</u>	<u>Scat</u>	<u>C.A.T.</u>	<u>Eagle</u>
GM	ROI-1-11	3-ICR-5700P	CR-350	-----
	ROI-1-13	3-ICR-5700	CR-350B	-----
FORD 302	ROI-2-15	3-ICR-5090P	CR-302 S	IR5090FP
		3-ICR-5090		SIR5090FB
FORD 351W	ROI-2-16	-----	CR-351W	SIR5956FP
				SIR5956FB
MOPAR	ROI-3-4	-----	CR-318	SIR6123CB

Any aftermarket, magnetic steel, rod bolt may be used. Cap screws are prohibited. Titanium rods and rod bolts are prohibited.

9.11.4 Oil Pump, Pan, and Cooler

Wet sump oil pumps only. Dry sump oil pumps are prohibited. Any after market oil pan, without an oil recovery pouch or power kickout on passenger side, may be used. External engine oil coolers are permitted. Coolers may not be located in the driver's compartment. Oil accumulators (Accusump's) may not be located in the driver's compartment. From front of oil pan 16" back 1" from top of pan 1" inspection hole on drivers side.

9.11.5 Cylinder Heads

Cast iron cylinder heads only. Must be O.E.M. Stock or O.E.M. replacement parts. (Allowable head numbers are 014, 195, 267, 330, 336, 339, 388, 393, 441, 445, 454, 487, 493, 545, 598, 624, 642, 709, 799, 813, 862, 881, 882, 920, 991, 993, 997) or any other OEM stock replacement head that fits these criteria. General Motors Vortec (Casting P/N 10239906 or 12558062) and SR cylinder heads are permitted. General Motors Vortec cylinder head P/N 25534351 & 25534371 are prohibited. Vortec heads may be drilled and tapped to install intake manifold.

No Dbl hump, angle plug, bowtie, W-2, GT-40 or SVO heads are allowed. SPECIALLY PRODUCED LIMITED PRODUCTION PARTS SUCH AS CORVETTE Z-28, OR MARINE APPLICATIONS TYPES ARE NOT ALLOWED! Aluminum cylinder heads are prohibited. Titanium valves are prohibited.

Cylinder heads must remain stock. All cylinder head markings must remain. Angle milling, chemical treating, acid dipping, acid flowing, abrasive blasting, bowl cutting, addition of material to the ports or combustion chamber, or other alterations to the original, as cast, head is prohibited. Valves, rocker studs, head bolts, and spark plugs may not be relocated. No polishing or grinding of ports or runners is permitted. No material may be added to the combustion chamber. The cylinder head to block surface may only be machined a maximum of 0.050 inches from OEM. A three angle valve job may be done as long as no machining marks are more than 1/8" above the head of the valve.

The maximum valve sizes, as measured across the face, are as follows:

<u>Manufacturer</u>	<u>Intake</u>	<u>Exhaust</u>
GENERAL MOTORS		
VORTEC	1.940 inches	1.500 inches
ALL OTHER GM	2.020 inches	1.600 inches
FORD "CLEVELAND"	2.046 inches	1.656 inches
FORD "WINDSOR"	1.8437 inches	1.5469 inches
MOPAR	2.020 inches	1.625 inches

The maximum allowable spring diameter is 1.32 inches. No beehive or double spring, and must appear OEM.

9.11.6 Camshafts, Valve Lifters, & Rocker Arms

Only flat tappet, steel, camshafts may be used. Rollerized camshaft bearings are prohibited. The maximum camshaft lift is 0.500 inches, measured at the valve retainer. Chain and sprocket camshaft drive system only. No roller tappets, or mushroom lifters are allowed. Valve spring retainers must be magnetic steel only. Only the following steel, straight barrel lifters are allowed:

<u>Manufacturer</u>	<u>Maximum Diameter</u>
GENERAL MOTORS	0.843 inches
FORD	0.875 inches
CHRYSLER CORP.	0.904 inches

Rev kits and stud girdles are prohibited. Only steel push rods are allowed. Only stock rocker arms are permitted. Roller rocker arms are prohibited.

9.11.7 Intake Manifold

Any cast iron, OEM, non high rise, intake manifold is permitted. Grinding or polishing of the ports is prohibited. Chemical treating, acid dipping, acid flowing, abrasive blasting, addition of material, or other alterations to the original, as cast, intake manifold is prohibited. General Motors intake manifolds 14096242, 14096011, 14097494, and 12366573 are prohibited.

Only the following, unmodified, one (1) inch, straight bore, phenolic adapter plates may be used between the intake manifold and carburetor:

Canton	Brzezinski
Racing	Racing
Competition Products	
#85-050	#108
#85-060	#105
#85-070	#101

NO chamfering, grinding, or drilling of the adapter plate is permitted. Only 2 gaskets (1 per side), with a maximum thickness of 0.065 inches, may be used.

9.11.8 Carburetor

Street Stock engines must run a Holley Model 4412S or 4412C two-barrel, carburetor. The carburetor must meet the following:

- A. Carburetor Body - No polishing, grinding, or drilling permitted. Factory type air bleeds only. Screw in air bleeds are prohibited.
- B. Choke - The choke may be removed.
- C. Choke Horn - The choke horn may not be removed.
- D. Boosters - The boosters and booster location may not be changed. The size or shape must not be altered. Boosters may not be tapered. Height must remain standard.
- E. Venturi - Venturi area must not be altered. Casting ring must remain.
- F. Base Plate - Base plate must not be altered.
- G. Butterflies - Butterflies must not be thinned or tapered. Retaining screws may not be altered.
- H. Throttle Shafts - Throttle shafts must not be thinned.
- I. Metering Block - Only metering block 134-137 is permitted. Adjustable metering blocks are prohibited. Metering block must not be altered.

Any carburetor can be claimed either for \$25.00 plus a carburetor of the same model from claimer's engine used in that race, or \$250.00 outright. Claims must be made, by the Competitor, to a Track Official in the Claim Area immediately after the completion of the division A Main race and be accompanied by cash or cashier check. Claims are limited to the top 5 finishing positions in the race. Claimee must have competed in A Main race. In order for the fifth place car to be eligible to make a claim, the fifth place car must have finished on the same lap as car which finishes in fourth place.

Any attempt to pull outside air other than down thru the venturies is prohibited. Throttle linkage must be solid rod, cable type linkage is prohibited. Two carburetor return springs, mounted in two directions, required. Gas pedal must be push/pull type. Toe loop required on gas pedal.

141 Speedway Engine Configuration (to be phased out in 2013)

- a. NO ALUMINUM HEADS OR ALUMINUM ENGINE BLOCKS.
- b. Must be on the same corporate family as the chassis.
- c. Welded stock and after market motor mounts allowed. MUST be in original position.
- d. Engines may be chained or strapped to restrict movement.
- e. Must be stock appearing. No external modifications allowed.
- f. Engines must be able to be used in a conventional passenger car without alteration
- g. Castings must not be changed.
- h. No machining on outside of engine block.
- i. Cast iron intakes and exhaust only.
- j. (Seymour Only) Exhaust pipe must extend beyond firewall. (Optional) muffler.
- k. Oil accumulators may be used but must be safely mounted. (Mounting subject to track tech approval)
- l. No electric fuel pumps or belt drive pumps.
- m. Fuel pump must remain in front of cross member or in stock location.
- n. Single OEM carburetor (two or four barrel) allowed. Holley 4412 allowed unaltered. (No Holley 4 Barrel allowed.)

Holley 4412 carburetor rules:

1. Carburetor Body – No polishing, grinding, or drilling permitted.
2. The choke may be removed
3. The choke horn may not be removed
4. The boosters may not be changed. The size or shape must not be altered. Boosters may not be tapered. Height must remain standard.
5. Venturi area must not be altered. Casting ring must remain.
6. Base Plate must not be altered.
7. Butterflies must not be thinned or tapered. Retaining screws may not be altered.
8. Throttle shafts must not be thinned.
9. Only metering block 134-137 is permitted. Adjustable metering blocks are prohibited. Metering block must not be altered.
10. Holley 4412 adapter plate maximum height 1 ¼" with gaskets
11. Two throttle return springs are mandatory on carburetor and must be attached using mounting brackets.

9.13 ELECTRICAL

Stock type distributor ignitions only with nonadjustable, non-multiple spark discharge. HEI type distributors must have the coil mounted in the distributor cap. Stock type module must be used. No external ignition boxes. Magnetos and crankshaft-triggered ignitions are prohibited. 12-volt battery and Electrical systems only. An ignition switch clearly marked on/off within reach of the driver is required. In addition to the ignition switch, a battery disconnect switch, mounted behind the driver and within reach of the safety crew, is also required. The disconnect switch must shut off all power to the car. The battery must be located behind driver's seat or in the trunk area, between the frame rails. If the battery is located in the driver's compartment, it must be enclosed in a container. If the battery is located in the trunk area, it must be between the frame rails and no lower than the bottom of the frame rail. The positive terminal must be covered. Timing advance knob allowed, must be external adjusting in engine compartment only! A recommended, marine type battery box to have a full steel plate bolted to floor and approved by track officials.

9.14 EXHAUST SYSTEM

O.E.M. unaltered cast iron exhaust manifolds only. All exhaust must extend past driver and exit under car. Exhaust pipe can be maximum size of 2.5 inch I.D.

9.15 COOLING SYSTEM

Radiators must be mounted in front of engine, not protruding thru hood, and can be any size. Aluminum radiators, water pumps, and pulleys are allowed. Electric fans allowed. All cars must be equipped w/ an approved overflow and catch can. Aftermarket or handmade fan shrouds are allowed. No aftermarket devices allowed that spray liquid on radiator. Water wetter is allowed.

No antifreeze or dex-cool allowed!

9.16 WHEELS AND TIRES

Aftermarket steel 15 inch X 8 inch racing wheels only. Minimum weight of 19lbs. Beadlocks allowed on right side only. A minimum of 5/8 inch wheel studs and 1 inch steel lugnuts are mandatory. Wheel spacers allowed up to one inch thick. Foam style or soft plastic mud plug allowed on right rear only. Right side tire pressures must be 15lbs. Pre-race to Post-race. Tires must be a Goodyear Eagle Short Track Special D3264-C400, D2599, D2342, D2345, D3268, 27 inch or 27.5 X 8 inch X 15 inch, Hoosier 26.5 inch X 8 X 15-500 and 27 inch X 8 inch X 15 inch-500. Tire reconditioning is allowed with a 16 grit or finer fiber disk with approval of Track officials.

G60 tire allowed. Siping allowed ONLY on the G60 tire.

9.16.1 TIRE DUROMETER LIMIT

Tire durometer limit: Up to 5 cars can be selected at random, prior to the start of qualifying races. Durometer readings will be taken at three points across the rear tires of each car. The readings will then be averaged together. The minimum allowable durometer reading of any tire prior to qualifying races, will be 90 percent of the average reading.

Example:

Average reading of 5 cars selected is 50, the minimum is 45.

9.17 FUEL

Gasoline or E85 only. Racing fuel is allowed. No performance enhancing or scented additives allowed. Track officials reserve the right to test any competitor's fuel at any time.

9.17.1 FUEL SYSTEM

Mechanical OEM push rod type fuel pumps only. All cars must be equipped with a racing fuel cell. The maximum capacity of the fuel cell is 22 gallons, and a minimum of 20-gauge steel. Fuel cell must be securely fastened in trunk area centered between rear frame rails, behind rear tires, no further forward than the factory seam where rear frame rail can be replaced and must have a metal firewall between driver and fuel cell.

Minimum height of bottom of fuel cell must be 12 inches at all times. The container must have (2) 1 inch by 1/8 inch steel straps around all 4 sides. Fuel cell vents including cap vent must have check valves. Fuel lines placed through drivers compartment must run through metal pipe or conduit. One fuel filter only and can not be placed in driver's compartment. Fuel cell vent hose must be below fuel cell.

9.18 MISCELLANEOUS EQUIPMENT

9.18.1 Seat

The seat must be a purpose built racing seat made of aluminum, and installed in a manner acceptable to Track Officials. It is recommended that the center of the seat be no less than 16 inches from the inside edge of the driver's side door bar. No less than 4, 1/2- inch diameter grade 5, bolts must be used to attach seat to roll cage. A flat steel washer no less than 1 1/2 inch in diameter must be installed between the head of the bolt and seat. Seat must be equipped with a padded cover. Headrest on seat is mandatory.

All roll cages must provide 2-inches, or more, clearance, measured from the bottom of the top halo bar of the roll cage to the top of the driver's helmet, when the driver is seated and strapped in, with the helmet on and in the driving position.

9.18.2 SAFETY – SEE SAFETY SECTION ON OPERATION RULES

Fire Control System: Car may be equipped with a fully charged fire extinguisher or on-board fire control system. A fully charged, 5 lb. minimum, ABC class, fire extinguisher must be provided and readily available in each pit stall.

Window Net: 1-inch web or knitted mesh window net on the driver's side. The minimum allowable length is 12 inches. The window net must attach to the roll cage at the bottom and release with a seat belt snap or Speed Zone approved release on the top front corner of the window. Window net must be in the up position any time the car is on the racetrack. Window net mounting subject to approval of track officials.

Tow Hooks: Front and rear tow hooks capable of supporting the weight of the car are mandatory and accessible to wrecker personnel.

Mirrors: Mirrors are prohibited.

Two Way Radios: All in-car communication devices are prohibited. A SW1600 model Raceciever, tuned to channel 0000 (454.000) is mandatory.

Transponder Location: Transponder must be placed twenty four inches (24"), or more, behind the center line of the rear axle to the forward edge of the transponder, on the right side frame rail. No metal of any kind can be between transponder and track. Transponder pouch should be placed horizontally with no obstructions between the pouch and the ground. If transponder pouch is mounted to round tube frame components with nylon ties, secure the transponder pouch so that it cannot spin from its horizontal position. This may be done by duct taping the nylon ties to the frame rail.

